



# golden downtown streetscape master plan

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## Chapter 1

### Introduction





Golden's downtown is a success story built on the **community's heritage, hard work, innovation,** and **entrepreneurial spirit.** Over the past 20 years, property owners, merchants, and the people of Golden have forged a partnership that has lead to the establishment of one of the **most successful downtowns in Colorado.**

While acknowledging the success of downtown, City Council felt it was important to create a vision that would **guide next 20 years of downtown progress** and commissioned this **Streetscape Master Plan** with that goal in mind.

## PURPOSE AND CHARGE

The mandate and goals for completing a Streetscape Master Plan for Downtown Golden are varied. They are derived from City Council directives, feedback from residents and the downtown community, and suggestions from the Golden Urban Renewal Authority (GURA) and City of Golden staffs. To be successful, the plan should:

- Provide a road map that can be used to guide decisions for enhancing downtown Golden's streetscape for the next generation.
- Generate excitement for taking the downtown to the "next level."
- Consider a wide range of options while being cautious about altering the streetscape's character and function that have made the downtown successful.
- Establish criteria that can be used to evaluate private requests to make changes to the streetscape.
- Envision design opportunities for the next generation of Washington Avenue.
- Create a more flexible and "useable" streetscape that allows for more opportunities for seating, gathering, festivals, and outdoor dining and retail displays.
- Evaluate moving the curb-line to widen the sidewalks to improve the pedestrian experience.
- Evaluate the pros/cons of revising vehicular parking and circulation.
- Create an environment that encourages bicycle access and which accommodates bike parking.
- Evaluate if making changes to Washington Avenue's brick and sandstone corner planters are appropriate to create a more useable streetscape.
- Look for ways to extend downtown beyond Washington Avenue and create "feeder spaces" for Washington Avenue retail.
- Evaluate streetscape enhancement opportunities for 12<sup>th</sup> and 13<sup>th</sup> Streets.
- Complete an evaluation of Jackson Street (12<sup>th</sup> to 14<sup>th</sup> Streets) to identify opportunities for enhancing the streetscape and encouraging redevelopment as defined in the East Downtown Plan.
- Develop a concept plan for Washington Avenue between 14<sup>th</sup> and 19<sup>th</sup> Streets that extends the appropriate design features from the northern blocks of Washington Avenue and improves pedestrian access.







Figure 1 - The Streetscape Master Plan study area



Figure 2 - Cast iron tree grates and brick pavers help establish the downtown's character



Figure 3 - Golden's arch is one of the most iconic downtown structures in Colorado

THE PLANNING PROCESS OVERVIEW

A collaborative public participation and outreach process was used to develop the Streetscape Master Plan. Responses from stakeholders and residents were used to evaluate the applicability of recent innovations in urban design and potential enhancements to the streetscape. The master plan process was completed over a seven-month period and provided multiple opportunities for residents, property owners, merchants, stakeholders, and city and GURA staffs to contribute the plan. This included:

- Small stakeholder group interviews with downtown merchants and property owners.
- A design charrette that featured an afternoon workshop to review new trends in urban design and downtown enhancement with members of the downtown community. This was followed by a design charrette with GURA staff, city staff, and the Master Plan Design Team.
- A public open house to review the concepts that evolved out of the initial public engagement and charrette process.
- A month-long review of the preliminary concepts on the city's website. MindMixer, an online community forum where constituents can "meet" to discuss issues and share ideas, was used to provide opportunities for residents to voice their opinions on the range of potential streetscape enhancement options.
- Throughout the process, City of Golden and GURA staffs were available to take comments and suggestions via phone and email.

THE STUDY AREA

The study area for the Streetscape Master Plan (Figure 1) incorporates the heart of downtown Golden along with several distinct character zones within the city center.

Washington Avenue from Clear Creek to 14<sup>th</sup> Street

This three and one-half block segment of Washington Avenue is what most people would consider to be "Downtown Golden". It features many of the architectural and streetscape details that establish the character for the downtown:

- Two to four story buildings, many of which are historic, framing an 80' right-of-way.
- Wide sidewalks with concrete and brick paver details (12' to 15' on wide on the east side and approximately 17' on the west side).
- The "Welcome to Golden" arch.
- Street trees in cast iron tree grates planted approximately every 30' to 40'.



- ↳ Curb extensions at each corner.
- ↳ Large, raised brick and rose sandstone planters on all but one corner; some with one or two ornamental trees and annual beds.
- ↳ Areas for outdoor dining where space allows.
- ↳ Decorative pedestrian-scale streetlights.
- ↳ Bronze sculptures -- most with a Western theme.
- ↳ Canopies supported by posts that extend over the sidewalk on a few of the buildings.
- ↳ An assortment of site furnishings and wayfinding signage.
- ↳ Two lanes of traffic with a center turn lane and parallel parking on each side of the street.

**Washington Avenue from 14<sup>th</sup> Street to 19<sup>th</sup> Street**

The most notable characteristics of Washington Avenue south of 14<sup>th</sup> Street are the very steep grade for the first block south of 14<sup>th</sup> Street and the large shade trees in tree lawns. This corridor features a mix of established residential, multifamily, and residential-scale commercial uses. The streetscape character is relatively consistent with:

- ↳ 5' to '6 concrete sidewalks separated by a 10' turf or shrub bed tree lawn on the west side.
- ↳ 4' to 6' attached concrete sidewalks on the east side.

**Jackson Street from 11<sup>th</sup> Street to 14<sup>th</sup> Street**

The streetscape for the three blocks of Jackson Street within the study area is the most varied in the downtown. Jackson Street is flanked by a mix of uses, many of which do not contribute the vitality of the streetscape. The finishes range from plain, narrow concrete sidewalks with a residential character to segments with a level of finish that is similar to the character of the buildings and streetscape on Washington Avenue. Jackson Street is defined by:

- ↳ Street trees in grates or planters on blocks between 13<sup>th</sup> and 11<sup>th</sup>.
- ↳ Sidewalks that vary from 4' to 15' in width and with finishes from brick pavers in curb extensions to plain concrete.
- ↳ At grade parking lots and bank drive-throughs.
- ↳ Residential-scale uses with grassy areas between the sidewalk and curb (west side of the southernmost block).
- ↳ Relatively narrow streets (+/- 48').

**11<sup>th</sup>, 12<sup>th</sup>, 13<sup>th</sup>, and 14<sup>th</sup> Streets**

The east-west streets within the study area run from Ford Street on the east to Arapahoe Street on the west.

- ↳ 12<sup>th</sup> and 13<sup>th</sup> Streets are more urban in character with streetscapes that typically include street trees in tree grates, brick banding at the back of curb and around tree grates, curb extensions with brick paving at Washington and Jackson Streets, and 9' wide sidewalks. There are a number of outdoor dining opportunities on the north side of 12<sup>th</sup> Street, east of Washington Avenue.
- ↳ 11<sup>th</sup> Street has a mix of commercial, and multifamily residential uses, and a parking structure. The streetscape character generally reflects the adjacent use with more urban details adjacent to the commercial development, turf grass and trees along the parking structure, and plain concrete adjacent to Ace Hardware. The public right-of-way for 11<sup>th</sup> Street ends at Jackson Street, turning to the south.
- ↳ Each of the east / west streets features curb extensions with brick paving and trees at the Washington Avenue intersections and, in some cases, areas for outdoor dining within the area of the extended curb .

**Miners and Prospectors Alleys**

Two, three-block alleys flank Washington Avenue from 11<sup>th</sup> to 14<sup>th</sup> Streets with Miners Alley to the west and Prospectors Alley to the east. Both alleys are very narrow with travel lanes that vary from 14' – 18'. They are primarily used for service and delivery for the Washington Avenue businesses and as pedestrian connectors between the public parking lots and businesses. Because the alleys are so narrow, they are difficult for pedestrians to negotiate when delivery trucks are present. The character of the alleys is dominated by service uses for adjacent businesses including dumpsters, overhead power lines, gas meters, and transformers. The asphalt paving in both alleys is in poor condition and a number of locations will need refurbishment in the future.

**Ford Street**

Ford Street is included with the study area primarily to help illustrate the overall vehicular circulation system for the downtown. Because of its width, Ford Street represents an opportunity to incorporate many of the “Complete Streets” principles in its design to make it a more people-friendly street.



Figure 4 - Sidewalk canopies provide shade and protection from rain and snow, but the support posts create obstructions



Figure 5 - Residential-scale streetscape on Jackson Street





Figure 6 - Context in the immediate vicinity of the Study Area

THE CONTEXT

The context for the downtown (Figure 6) creates a unique physical setting.

- ↪ The foothills form the western edge of the immediate context, Clear Creek the north edge, and South Table Mountain the east edge. These features are an integral part of downtown Golden’s character and create a sense of enclosure for the study area.
- ↪ The proximity of the mountains and foothills to the west of the downtown means the sun sets earlier than areas further to the east. Snow can linger longer and it also means cooler temperatures for outdoor uses in the evenings.
- ↪ Since there are only two vehicular bridges crossing Clear Creek, opportunities to change traffic patterns in downtown Golden are limited, especially for north / south streets. The topography within the study area and the immediate context influences the downtown in a number of ways:
  - Because of fairly steep south to north grades within the study area, stormwater flows have more velocity and therefore can be more difficult to manage.
  - Washington Avenue also slopes from west to east, which means that many of the grades for sidewalks and the road surface are close to the allowed maximums. This makes it difficult to make significant changes such as eliminating curb and gutter without detailed engineering plans.
  - The southern edge of the Clear Creek corridor creates a steep hill-side that makes it difficult to link the core of the downtown to the Washington Avenue neighborhood south of 14<sup>th</sup> Street.

There are a number of important uses in the immediate context of the study area.

- ↪ The Colorado School of Mines (CSM) campus runs along the southern two-thirds of the west side of the study area. The School of Mines has a significant impact on the downtown by creating demand for restaurants and lodging along with increasing demand for parking - especially in the southwest quadrant of the downtown and on Washington Avenue south of 14<sup>th</sup> Street.
- ↪ There are number of MillerCoors properties east of Ford Street including the Wellness Center and the parking lot for the brewery tour. The MillerCoors brewery attracts over 250,000 visitors to downtown Golden each year and it is an on-going goal to entice as many of these visitors to the downtown as possible before and after their tour.
- ↪ There are established residential neighborhoods to the northwest, south, and southeast of the study area.



- ↪ The City’s government complex is located northwest of the downtown on the north side of Clear Creek.
- ↪ Parfet Park is immediately north of the study area and Clear Creek, and is the site for a number of major festivals during the year.

Each of these features has had a significant impact on the character and success of downtown Golden and on the shape of the recommendations for this Streetscape Master Plan.

THE EVOLUTION OF THE DOWNTOWN

The history of downtown Golden dates back to the 1860’s when Golden City was established during a gold rush. As an important gateway to the Rocky Mountains, Golden was briefly the capital of the Colorado Territory. A number of significant buildings from this period still exist on Washington Avenue. The city’s population and the downtown’s prosperity ebbed and flowed throughout the 1900’s. With the post World War II change in development patterns and competition from suburban shopping malls, the downtown declined throughout the 1970’s and 1980’s. In the early 1990’s the downtown community realized that redevelopment was needed. The current character of downtown Golden was then established through a major investment in the streetscape and the surrounding buildings. The brick paving, curb extensions at the corners, street trees, and large, raised brick and sandstone corner planter were all a result of the 1992 Downtown Redevelopment Project.

Recent Accomplishments

The downtown has continued to grow and evolve over the last 20 years. Some of more notable changes include:

- ↪ The addition of two parking garages on the west side of Jackson Street.
- ↪ Multifamily housing projects east of Washington Avenue, north of 11<sup>th</sup> Street, and at the northeast corner of Washington Avenue and 13<sup>th</sup> Street. The downtown has added over 200 residential housing units within the core area in recent years.
- ↪ Construction of the Table Mountain Inn and the Golden Hotel.
- ↪ The addition of mixed use office and retail on the north side of 12<sup>th</sup> Street east of Washington Avenue. The number of office workers in the downtown recently saw a dramatic increase with the addition of Source Gas as an employer in the downtown.

- ↪ Continued evolution of the downtown market as new businesses are added or converted to a different use. For instance the number of restaurants has increased the last 10 years and the Foss Drug building was converted from a single user into several new uses.
- ↪ The extension of streetscape enhancements along 12<sup>th</sup> and 13<sup>th</sup> Streets and portions of Jackson and 11<sup>th</sup> Streets.

HOW DOWNTOWN GOLDEN’S CURRENT STREETScape IS VIEWED

Throughout the public engagement process, participants were asked their views on “What they like best” and “What they like the least” about Golden’s downtown. Feedback was collected from downtown merchants and property owners, study area residents, and the community as a whole on how the existing streetscape in the downtown is perceived.

Open House and MindMixer Results - “Like Best” and “Like Least”

The initial community open house took place in July, 2013. The open house format included a station where participants could comment on the existing conditions of the current streetscape in downtown Golden (Figure 7 - on the next page). To supplement the feedback received at the open house, MindMixer, an online tool where ideas were posted and interested parties could comment, was used to help engage additional members of the community about existing conditions of the streetscape. Table 1 displays the topic and the corresponding choices. Participants were also encouraged to comment on additional aspects about the streetscape that should be considered in the development of the plan.

Due to limitations with the number of options that could be presented on the MindMixer site, some of the choices presented at the open house were combined in the MindMixer version. For example, in the “liked best” category, large shade trees, brick planters, and Western-themed art were combined into “the details and materials of the rich streetscape”; availability of parking was combined with convenient on-street parking. For “liked least”, not enough room for comfortable public seating, sidewalks not wide enough, brick planters taking up too much room, and canopy posts inhibit pedestrian flow were combined into “items blocking the sidewalk”.

Like Best

The choices that participants (combining the results from the open house and MindMixer formats) indicated they liked best about the downtown Golden’s streetscape were:

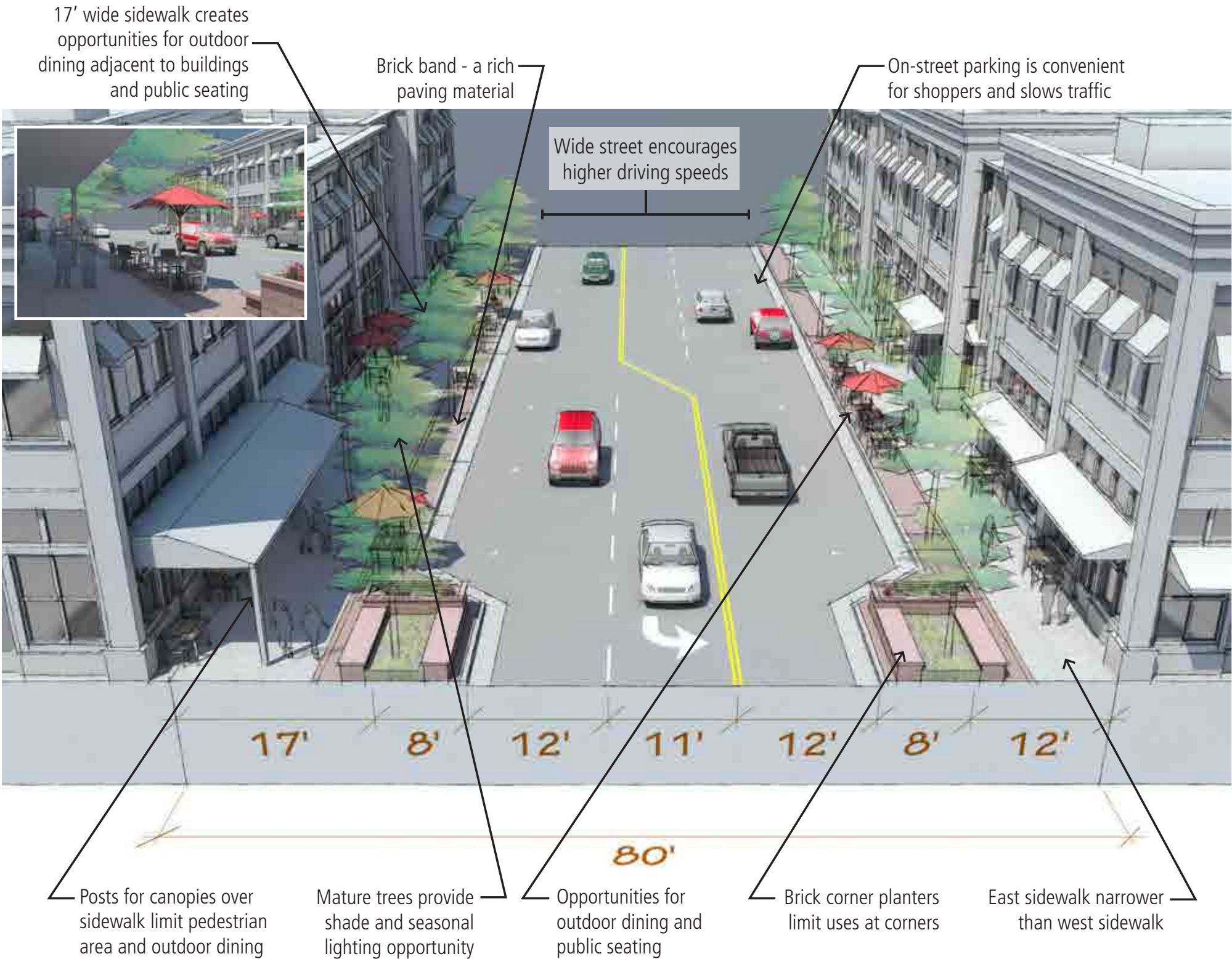
- (1) The rich streetscape details and materials.
- (2) The availability of seating for outdoor dining.
- (3) Wide sidewalks.

| What I like best about the existing Washington Avenue streetscape is... | What I like least about the existing Washington avenue streetscape is... |
|---|--|
| The rich details and materials*   | Too much emphasis on cars (e.g. wide street, on street parking)          |
| Convenient on-street parking  | Not enough room for comfortable public (non-dining) seating*             |
| Large shade trees*  | Large shade trees block store signage                                    |
| Wide sidewalks  | Sidewalks are not wide enough*   |
| Brick planters on the corners*  | Brick planters on the corners take up too much room*                     |
| Western-themed art*   | Too much Western-themed art  |
| Availability of seating for outdoor dining                              | Posts for sidewalk canopies inhibit pedestrian flow*                     |
| Business directory signage and wayfinding                               | The pedestrian crossing signals don’t seem to be efficient               |
| Availability of parking   | Items blocking the sidewalk (e.g. sandwich board signs, seating, art)*   |
|   | Business directory signage and way-finding                               |
|   | Availability of parking  |
| * combined in MindMixer version   |  |

Table 1: Most frequent public Responses from the open house and MindMixer review



Figure 7 - A prototypical representation of Washington Avenue's existing conditions (looking north)



The rich streetscape details and materials – including large shade trees and brick planters – was a popular choice with participants of both formats. However, some variation did occur between the open house responses and the MindMixer site. The availability of outdoor seating and for wide sidewalks was much more popular for participants using the MindMixer format than those collected at the open house. At the open house, neither option was selected as one of the top three choices. However, as result of the MindMixer responses, they availability of outdoor seating and wide sidewalks was one of the top three overall choices selected. Interestingly, the availability of parking was not selected by any participants as an aspect that they liked best.

*Like Least*

For aspects that were liked least, the most selected options were:

- (1) Items blocking the sidewalk.
- (2) Too much emphasis on cars.
- (3) The pedestrian signals do not seem efficient.

Again, there were slight differences between the open house results and those from the MindMixer version. However, the top three selections for both versions were the same, just in a different order.

*Comments Received*

Participants were encouraged to comment on the choices and add others that were not listed. Many comments indicated that trees are a positive aspect of the downtown and should not be considered negative despite the argument that they may block views to business signage. Many commenters also wanted to make the streetscape more pedestrian friendly by replacing parking with sidewalks or seating, removing obstacles, and reducing traffic congestion. Others wanted to see more design standards for private street furniture and better overall maintenance of landscaping and paving.

As a result of this exercise, the direction for new streetscape improvements was to provide options that:

- ↪ Balance the emphasis on cars with that of the pedestrian.
- ↪ Make pedestrian mobility to and through the downtown easier and more convenient.
- ↪ Open up the sidewalks and create more space for public use.
- ↪ Build on the existing details and design.
- ↪ Maintain or expand the availability of outdoor dining.





## Chapter 2

### Assets, Opportunities, and Challenges







Figure 8 - Western themed art can be found on most street corners in the downtown



Figure 9 - Effective use of clay pavers, brick veneer and sandstone

## ASSETS AND OPPORTUNITIES – WHAT’S WORKING WELL IN THE DOWNTOWN?

The growth of the downtown since the 1992 renovation is evidence that the streetscape enhancements have served as an important catalyst. Golden’s downtown is a benchmark by which other small cities measure their downtowns. This is not to say that the city’s core has reached its full potential. There is certainly room for growth and to create a more complete urban center. The framework for this next phase of the downtown’s evolution is already in place. There are a wide range of assets and opportunities upon which to capitalize, and there are challenges that will need to be addressed – all of which will shape downtown’s pathway to the future.

### Identifiable Character

Golden’s downtown is immediately recognizable and memorable for its visitors. The City of Golden has taken great pains to protect and enhance this identity and there is strong support among the downtown stakeholders to not deviate from our “brand”.

- ↪ There is an interesting mix of architectural styles and details in the downtown. The historic buildings establish the architectural character and, for the most part, new buildings have been designed with materials and architectural styles that are compatible with the existing architectural fabric.
- ↪ The “Welcome to Golden” arch is one of the most iconic downtown structures in Colorado and it is distinctively Golden. The painted black steel has been repeated throughout the downtown further strengthening the arch’s importance to the downtown streetscape.
- ↪ The streetscape materials create a brand that is unique to Golden:
  - The use of clay paving bricks.
  - A mix of brick and rose sandstone in planters and walls.
  - Western art throughout the downtown.
  - A strong framework of street trees in cast iron tree grates.

### Adequate Parking

While the perception is often quite different, studies have shown that there is more than adequate parking available to visitors and shoppers in the study area.

- ↪ Washington Avenue offers parallel parking on both sides of the street in the downtown core. These spaces are usually filled during peak hours, which creates friction, slows traffic, and conveys an impression of a busy, vibrant retail core. On the other hand, it also gives the impression that parking is limited in the downtown.
- ↪ There is a mix of parallel and angle parking on the other streets in the study area. There is potential to increase the amount of parking on these streets by converting parallel parking to head-in or back-in angle parking.
- ↪ There are two parking structures on the west side of Jackson Street, which offer free parking and are rarely full. The structure north of 13<sup>th</sup> Street is fully dedicated to public parking while the structure north of 12<sup>th</sup> Street is a public / private split. New visitors often resist using parking structures because there is a perception that there will be a fee to park. This can be overcome with clear signage.
- ↪ The two public surface lots located west of Washington Avenue are heavily used. There is a perception among downtown merchants that the lot between 13<sup>th</sup> and 12<sup>th</sup> Streets (the Foss lot) is often used by CSM students to access the campus, limiting its use for shoppers.

### Ongoing Investment

The City of Golden and private property owners continue to invest in the downtown’s success. A number of properties have been redeveloped over the last 20 years throughout the study area. The most notable examples include:

- ↪ Gateway Station at northeast corner of Washington Avenue and 13<sup>th</sup> Street. This is a 5-story mixed-use redevelopment with 35 residential units and over 16,000 square feet of commercial space. Gateway Station’s streetscape matches Washington Avenue’s materials and details. Parallel parking spaces were removed on 13<sup>th</sup> Street to construct a large, successful outdoor dining area on the south side of the building.
- ↪ Jackson Court (adjacent to the parking garage on Jackson and 12<sup>th</sup> Streets) is a two-story office / retail building with raised patios for outdoor dining and brick planters fronting on 12<sup>th</sup> Street.
- ↪ The Clear Creek Square Office Building is on the north side of 12<sup>th</sup> Street between Jackson and Ford Streets. This building runs the length of the block and provides outdoor patio space between the sidewalk and the façade.



- Millstone Clear Creek Square contains three, 4-story loft-style buildings with 78 residential units and underground parking. It is located between Clear Creek and 11<sup>th</sup> Street.
- The Table Mountain Inn worked with the City and paid for the construction costs to remove parallel parking spaces in front of its restaurant on the west side of Washington Avenue. This allowed them to maintain wide sidewalk add an outdoor dining patio.
- The parking structures on Jackson Street represent significant investments by the City and GURA and include streetscape enhancements.
- The City of Golden and GURA have enhanced the streetscape for 13<sup>th</sup> Street between Washington Avenue and Ford Street with street trees in tree grates, brick paving bands, and a gateway arch at the intersection of 13<sup>th</sup> and Ford Streets.

Opportunities for Growth

There is a strong mix of retail, restaurant, and service uses in Golden’s core. However, there are properties within the study area that will likely see redevelopment as Golden grows.

- Several properties east of Washington Avenue, primarily along Jackson Street, could be redeveloped from lower density uses to higher, more urban uses. A mix of housing, office, and commercial that would support the historic downtown should be encouraged and streetscapes for redeveloping properties should be planned to encourage a vibrant public realm.
- The surface parking lots throughout the study area represent opportunities for future structured parking or a combination of higher density development with structured parking on the lower levels and ‘liner’ retail fronting on the streets.
- While it is not feasible to permanently close Washington Avenue to traffic, it may be possible close other streets in the study area (or portions of other streets). Temporary closures of these streets for special events are certainly possible and may be preferable to closing Washington Avenue. Permanent closures may also be possible, depending on the street in question.

Programs to Activate the Downtown

The City of Golden has done an excellent job of creating a year-round calendar of special events that draw residents and visitors to the downtown.

- There are approximately 10 large special events hosted in and around the downtown each year. Some of the most popular include the Candlelight Walk during the holidays, Buffalo Bill Days, and monthly First Friday Street Fairs in the warmer months.
- There are additional events scheduled on a regular basis in the downtown, typically during the summer months. This includes various parades and street events and Movies and Music in the Park.

Other Opportunities

- The City has an ongoing maintenance program where streets are resurfaced before serious degradation occurs. Washington Avenue is schedule to be resurfaced sometime in the next two to three years. Any enhancements to Washington Avenue’s roadway that evolve out of the Streetscape Master Plan process should be evaluated before resurfacing is completed.
- A steam tunnel runs west from the MillerCoors brewery to the School of Mines along 14th Street. It may be possible for the City to reach an agreement whereby this waste heat is also used to create a snow melt system for the downtown streets and/or sidewalks.

*Many special events held in Downtown Golden require full closure of one or more streets in the downtown, which can be problematic given the limitations of the downtown street grid. Since requests to host festivals in downtown Golden are submitted on a regular basis, additional venues for special events in the downtown beyond Washington Avenue would provide opportunities for smaller or start-up festivals.*

*Liner retail can take many forms. They can be shallow niches in the facade of a building no deeper than a small vendor kiosk that provide opportunities for local and small scale businesses. They can also be the depth of a parking stall (+/- 18’) if combined with a parking structure.*

*(Image courtesy of David Baker)*



Figure 10 - Attractive shops and dining decks on 12<sup>th</sup> Street





Figure 11 - Steep slopes on Washington Avenue



Figure 12 - Sidewalk canopy posts limit use of the sidewalk

## CHALLENGES – WHAT NEEDS TO BE IMPROVED?

### Expanding the Downtown

During the public engagement process, participants frequently expressed a desire to expand the downtown beyond Washington Avenue so that Golden is not a “one street town”. In the last 10 years there has been notable expansion of downtown beyond Washington Avenue, primarily along 12th Street. While this is great beginning, more work is needed to create a complete urban center.

- Redevelopment of underused properties east of Washington Avenue is important for future of the downtown. But, because private initiative is required, it may take a number of years for these properties to redevelop.
- Enticing shoppers to leave Washington Avenue and explore and visit new restaurants and shops on 12<sup>th</sup> Street has proven to be a challenge. Additional streetscape enhancements may be needed to increase pedestrian activity.
- The Wells Fargo Wire Transfer building at the northeast corner of 13<sup>th</sup> and Jackson Streets does not contribute to the vitality of the downtown. It presents blank facades to the streets and the only users are the employees. Finding creative solutions to minimize impact of this use is an ongoing challenge.

### Steep Grades

While not obvious to the average user, the grades in the downtown core are actually quite steep when compared to most downtowns. Steep grades create design challenges for drainage, public gathering spaces, and, in some areas, for pedestrian movement.

- The grades on Washington Avenue immediately south of 14<sup>th</sup> Street are exceptionally steep and can discourage pedestrians from walking to the south beyond 14<sup>th</sup> Street.
- There are a wide range of engineering challenges to making changes to the curb lines and storm drainage system on Washington Avenue from Clear Creek to 14<sup>th</sup> Street. Significant changes in this area will require detailed engineering to determine cost and feasibility.
- Adding outdoor dining on paving with slopes over 2% make providing level tables and seating difficult.
- Because of strong summer rain storms, the streets in the downtown need to convey heavy flows at times. Adding wood dining decks at the curb line (similar to the one installed at Woody's) is discouraged by City Engineering staff. The concern is that storm debris will get caught under the deck and block or divert storm flows.

### Washington Avenue (Clear Creek to 14<sup>th</sup> Street)

There a number of design challenges unique to the established blocks of the downtown between Clear Creek and 14<sup>th</sup> Street.

- A number of people suggested closing Washington Avenue to vehicular traffic to create a pedestrian mall. Until an additional bridge crossing is provided over Clear Creek, this is not feasible due to lack of options for accommodating north-south traffic flows and emergency vehicle routes. Staff recommends that even starting a closure south of 11<sup>th</sup> Street would not be feasible without disrupting many properties with a through route connection to Jackson Street for southbound traffic and to Ford Street for northbound traffic. Such a permanent proposal would also challenge Ford Street's capacity.
- The sidewalk on the east side of Washington Avenue is +/- 5' narrower than the west side, limiting opportunities for outdoor gathering spaces and seating.
- The large brick and stone planters on the Washington Avenue's corners are a part of the downtown Golden's identity, but they inhibit seating and gathering opportunities. They also limit the ability for businesses with frontage along the planters to provide outdoor retail displays or dining.

### Sidewalk Canopies

Canopies extend out from the facades of a number of the Washington Avenue buildings notably part of both sides of the 1200 block and at the Buffalo Rose. These canopies are typically wood construction, extend 7' to 8' over the sidewalk, and are supported by wooden posts. While they do have an Old West flavor, the support posts limit the types of uses that can be accommodated on the sidewalks, especially on the narrower east side sidewalks. If merchants and property owners feel that the canopies are necessary, options should be explored to support them from the buildings and eliminate the posts in the sidewalk.

### Shortfall in Public Seating / Bike Racks / Lack of Consistency

While benches are provided in the study area, there is a serious shortage of comfortable places for patrons to sit in the public right-of-way. There are only few benches provided in each block. The sandstone caps for the corner planters are at the correct height to sit on in a few areas, but these are not as comfortable as dedicated seating, especially for individuals with limited strength or mobility since there are no armrests. Additional public seating should be provided.



There were comments made during the public engagement process about the limited accommodations for bikes in the downtown. The City's current policy of having bikes "share" streets with vehicles is appropriate for a downtown. It is consistent with current planning principles for bike use in urban areas and has the added benefit of slowing traffic. There is a shortage of bike racks throughout the study area. Additional bike racks should be provided.

There is no consistency of materials for the site furnishings in the downtown. The City should consider adopting a single system of benches, bike racks, and where not incorporated into the corner planters, trash receptacles.

**Other Challenges**

A number of issues were identified by members of the downtown community and residents during the planning process that are not directly related to streetscape design.

- ↳ Business owners were concerned about Colorado School of Mines students and staff occupying parking spaces for extended periods of time – especially in the parking lots west of Washington Avenue.
- ↳ The mix of uses in the downtown was a concern. Many felt that more retail and restaurant uses are needed in lieu of the existing service uses to create the "density" a successful downtown requires.
- ↳ Accommodating delivery vehicles in the downtown without impacting shoppers and pedestrians is an ongoing challenge in any community. This is especially problematic if Miners and Prospectors Alleys are to be used for business access, retail, or outdoor dining.
- ↳ The current system of incorporating trash receptacles in the brick corner planters has serious drawbacks. The size of the containers means maintenance staff has to empty the receptacles frequently and there is no opportunity to add recycling receptacles. The City should look closely at moving away from the trash receptacles that are built into the planters to a system that combines trash and recycling. There are a wide range of trash/recycling systems with double (trash / recycle) and triple (trash, bottles & cans, and paper) receptacles available. City maintenance staff has expressed an interest in a system that compacts the waste in-place, reducing the frequency of having to empty the units. The cost / benefit of this type of waste system will need to be evaluated along with long-term durability before it is adopted as the standard.



Figure 13 - Deliveries make it difficult for pedestrians to use Miners Alley



Figure 14 - Trash receptacle built into corner planter







## Chapter 3

### Initial Public Outreach





This chapter provides an overview of the key insights contributed by stakeholders and residents who were involved in the preliminary phases of the planning process. The two primary public outreach efforts at the commencement of the project were stakeholder meetings and a design charrette. Understanding that each and every comment cannot be included, this chapter aims to highlight common preferences that were mentioned by participants at these venues.

## STAKEHOLDER MEETINGS

Two stakeholder meetings were conducted early in the process to collect ideas and identify issues and constraints for the downtown streetscape's enhancement. The meetings were designed to include a representative cross section of downtown merchants and/or property owners. The goal was to understand what they like about the Downtown and what they'd change. In addition to the dialogue, participants ranked potential improvements that would help achieve their vision of the downtown on a scale from 1 to 5 (where 1 means "not at all important" and 5 means "very important"). 12 stakeholders participated in the two meetings.

### What We Heard

#### Stakeholder Dialogue

During the dialogue, the stakeholders voiced their opinions and responded to questions about the downtown. The topics covered included increasing activity in the downtown, pedestrian and vehicular circulation, and aesthetics.

In regard to increasing activity, stakeholders wanted to see more events and attractions that brought people into the downtown. This included more retail uses, restaurants, and night-time activities, as well as additional parades and festivals. Participants also envisioned the expansion of downtown businesses onto side streets to create a larger downtown area. Wayfinding and event signage was presented as a way to better inform people on how to get to and around downtown as well as to inform patrons about current and upcoming special events.

The stakeholders felt that a constraint to creating more activity in the downtown was the lack of gathering spaces and sidewalks with obstructions. The posts for the sidewalk canopies, brick corner planters, sandwich boards, outdoor retail displays, and on-street parking were cited as a hindrance for

public use of the sidewalk. However, the importance of on-street parking to business owners was also acknowledged.

Both pedestrian circulation and vehicular circulation were discussed. Stakeholders pointed out areas of conflict between bicycles, cars, and pedestrians. The amount of traffic in general and difficulty of getting around by car were concerns. Participants said bicycles needed to be better accommodated as they are a popular mode of transportation but often cluttered the sidewalks.

In regard to aesthetics, some participants indicated that they wanted to see higher standards for the outdoor dining areas to create a more pleasing, cohesive atmosphere. Others noted that streetscapes along side streets should be made consistent with Washington Avenue's.

## DESIGN CHARRETTE

Shortly after the Stakeholder Meetings, a group of 20-30, including stakeholders, City staff, and representatives from the Master Plan Team participated in a one-and-a-half-day charrette. Tasks accomplished at the charrette included:

- Review goals and expected outcomes of the charrette.
- Review existing conditions in the study area.
- Discuss the input received at the Stakeholder Meetings.
- Conduct a "visioning" exercise which included a presentation on re:Streets principles. The re:Streets approach is based on a design philosophy that pushes beyond current standards to explore the future of streets and what America's roadways would be like if they were designed for living, instead of just driving.
- Work sessions held with city staff and the Master Plan team to develop and refine streetscape design options.

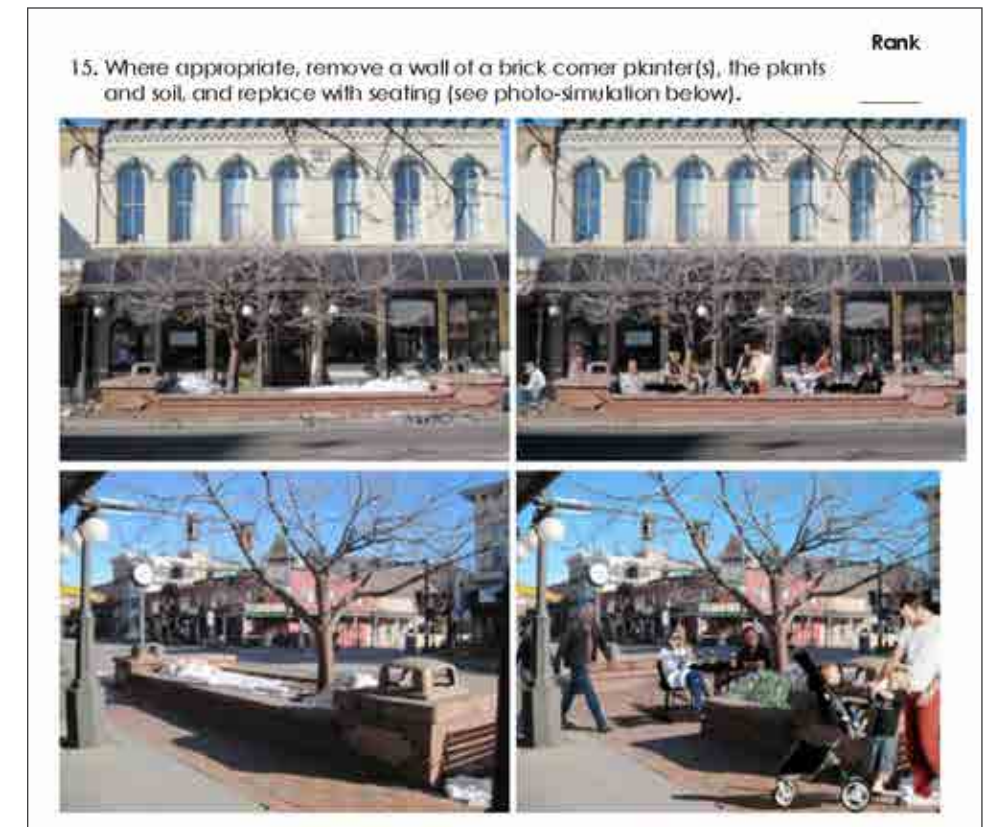


Figure 15 - Stakeholders were asked their thoughts on the corner planters



Figure 16 - Brick planters and posts limit sidewalk gathering spaces

|                               | Shared Streets | Naked Streets | Liner Retail | Flexible Streets | Art and Interactive Structures | Signature Multi-seasonal Streets | Reclaiming Right-of-way | Pavement to parks | Green Infrastructure |
|-------------------------------|----------------|---------------|--------------|------------------|--------------------------------|----------------------------------|-------------------------|-------------------|----------------------|
| Washington Avenue             | ✓              | ✓             | ✓            | ✓                | ✓                              | ✓                                |                         | ✓                 | ✓                    |
| Jackson Street                | ✓              | ✓             | ✓            | ✓                | ✓                              |                                  |                         | ✓                 | ✓                    |
| 11 <sup>th</sup> Street       | ✓              | ✓             | ✓            | ✓                | ✓                              |                                  | ✓                       | ✓                 | ✓                    |
| 12 <sup>th</sup> Street       | ✓              | ✓             | ✓            | ✓                | ✓                              |                                  |                         | ✓                 | ✓                    |
| 13 <sup>th</sup> Street       | ✓              | ✓             | ✓            | ✓                | ✓                              |                                  |                         | ✓                 | ✓                    |
| 14 <sup>th</sup> Street       | ✓              | ✓             | ✓            | ✓                | ✓                              |                                  |                         | ✓                 | ✓                    |
| Miners and Prospectors Alleys |                |               | ✓            |                  | ✓                              |                                  |                         |                   | ✓                    |
| Ford Street                   | ✓              |               |              |                  |                                |                                  |                         |                   | ✓                    |
| Neighborhood                  |                | ✓             |              |                  |                                |                                  |                         |                   |                      |

Table 2: Creative streetscape concepts downtown stakeholders said were appropriate for downtown Golden

re:Streets Presentation and Key Pad Polling

One of the key tasks of the charrette was to identify appropriate streetscape concepts for downtown Golden with the aim of having Washington Avenue and the downtown function better as a public space. The group viewed a presentation on how streets can be altered so that they can be used in different and new ways. In response to the various concepts, participants indicated the appropriateness of the ideas via key pad polling. Key pad polling allows participants to selected answers to questions about each concept and see a graphic summary of the group’s the responses in the next slide. The complete results of the key pad polling exercise can be found in the appendix.

Review of re:Streets of Concepts

The group indicated that there was a high potential for the concepts of:

- ↳ Shared Streets –Shared streets are designed to equally accommodate motor vehicles, bicycles, and pedestrians. Each mode of transportation has designated areas within the streetscape, which are often separated from the other modes. The streetscape includes wide sidewalks and crosswalks, bicycle lanes, and traffic travel lanes. Shared streets also typically provide areas for sitting and public gathering, areas to accommodate festivals and special events, and parking for both vehicles and bicycles.
- ↳ Naked Streets – The naked streets approach is intended to reduce the dominance of vehicular traffic and increase the mobility of the pedestrian. This is done by removing features such as traffic signs, road surface markings, and other informational signage. The street is then shared by all modes of transportation where the users must interact and negotiate in order to move about the space, thus, leading to a higher level of road safety.
- ↳ Liner Retail – Liner retail can lead to increased vitality on the street by offering more pedestrian accessible retail in booths and other small structures or vehicles within the right-of-way or other public space. Popular retail might include food vendors, clothing or gifts.
- ↳ Flexible Streets – Flexible streets allow temporary or partial closures for public gathering spaces and/or special events. This concept is to use policy, practice, and street design to move away from catering to the automobile based on where the street is located (e.g. the downtown) and the intended function of the area (e.g. public gathering space). It combines context-sensitive design and traffic calming with the establishment of broad objectives and purposes for main streets. This concept would allow portions of the street to be changed from vehicle-based uses to uses such as public seating, outdoor dining, bike parking. It also allows the streets in the downtown to be temporarily closed down to allow for special events.



- Art and Interactive Structures – Art and interactive structures, located in the right-of-way or other public space, add to the enjoyment of the Downtown by providing alternative activities based on sensory experiences (touch, sight, sound, etc.) to the traditional shopping and dining experience. Examples include splash pads, night lights influenced by body movement, structures that response to touch with light displays, and laser shows.
- Green Infrastructure – The application of green infrastructure, where nature and natural systems are used to address urban systems was also supported, particularly for water management and energy generation. Other green infrastructure concepts could be applied to material production, waste management, and habitat creation / restoration.

The concepts that were thought to have less of a potential for downtown Golden include:

- Pavement-to-Parks – Pavement-to-parks is a concept where an underutilized area within public rights-of-way is temporarily reclaimed to become a new plaza, seating area, or other public gathering space. To transform the area, the pavement is painted or treated to distinguish it from the traffic travel lanes and protective barriers are placed along the periphery.
- Reclaiming Right-of-Way for Community Gardens – An alternative to pavement-to-parks concept, excess right-of-way could be transformed into an area where community gardens could be developed by removing the asphalt and road base or installing raised beds. The concept encourages urban agriculture where food is grown and distributed within the city.
- Signature Multi-Sensorial Streets – Multi-sensorial streets are designed to engage users to use all their senses when visiting the area. Lights, graphics, and structures are used to attract the eye; music or other sound is added for the ears; and a variety of textures and materials are used in structures and art to encourage touch.

Gaining public support was the common response to the challenges that could block implementation of each of the concepts, although it was acknowledged that there may also be engineering issues in some cases and limitations on available funding.

### *Appropriate Location of Concepts*

As to where concepts should be applied, participants indicated that the concepts with a high potential could occur on most streets.

- Shared streets - Half of the respondents indicated that the shared streets concepts should be applied to all streets (Jackson Street, Washington Avenue, Ford Street, and the east-west streets) in the downtown; the second highest response was to apply the concept to Jackson Street.
- Naked streets – The highest response for naked streets was to have the concept applied to streets in both the neighborhoods leading to downtown and those in and around the downtown core.
- Liner retail - The majority of participants felt that liner retail would be appropriate on all streets (Jackson Street, Washington Avenue, the east-west streets, and Miners and Prospectors Alleys).
- Pavement-to-parks – The highest responses for where pavement-to-parks could occur was either on Jackson or all streets (Jackson Street, Washington Avenue, 11<sup>th</sup>, 12<sup>th</sup>, 13<sup>th</sup>, and 14<sup>th</sup> Streets).
- Flexible streets – The group felt that temporary, partial closures for public gathering spaces and/or special events was applicable on all streets (Jackson Street, Washington Avenue, 11<sup>th</sup>, 12<sup>th</sup>, 13<sup>th</sup>, and 14<sup>th</sup> Streets).
- Art and Interactive Structures - Art and interactive structures was felt to be appropriate for all streets in the study area.
- Green infrastructure – The participants indicated that green infrastructure should be applied to all downtown streets.

Participants were less specific for the locations of signature multi-sensorial streets. These might be appropriate for Washington Ave followed by east-west streets although some participants felt the concept was appropriate for all downtown streets (Jackson Street, Washington Avenue, 11<sup>th</sup>, 12<sup>th</sup>, 13<sup>th</sup>, and 14<sup>th</sup> Streets, Miners and Prospectors Alleys). There was less consensus for reclaiming right-of-way for community gardens but, if it was applied, 11<sup>th</sup> Street could be a candidate.



Figure 17 - Keypad Polling response slide



Figure 18 - Interactive art was supported by stakeholders, the "Hydraulophone" at the Ontario Science Center is an interesting example

**Ranking Exercise**

The participants at both the stakeholder meetings and the design charrette were asked to indicate the importance of a variety of improvements that would help achieve their vision for the Downtown. The complete ranking summary can be found in the appendix.

The improvements receiving the highest ranking revolved around places to gather and sit and to encourage more business development on streets adjacent to Washington Avenue. The improvements that received the highest combined rankings included:

- ↪ Where appropriate, remove a wall of a brick corner planter, the plants and soil, and replace with seating.
- ↪ Improve Jackson Street and side streets to encourage business development.
- ↪ Create more public gathering spaces on Washington Avenue.
- ↪ Create more opportunities for seating on Washington Avenue.

There was less support for making Jackson Street an event street or adding more Western art. Improvements that received a low ranking for importance included:

- ↪ Make Jackson Street an event street that could be closed to traffic.
- ↪ Make Jackson Street an event street that is closed to traffic except for essential destinations such as the parking garage or ends of each block.
- ↪ Improve the streetscape for Washington Avenue south of 14th Street.
- ↪ Add more public art in keeping with the Western theme.

**HOW THE INFORMATION COLLECTED WAS USED**

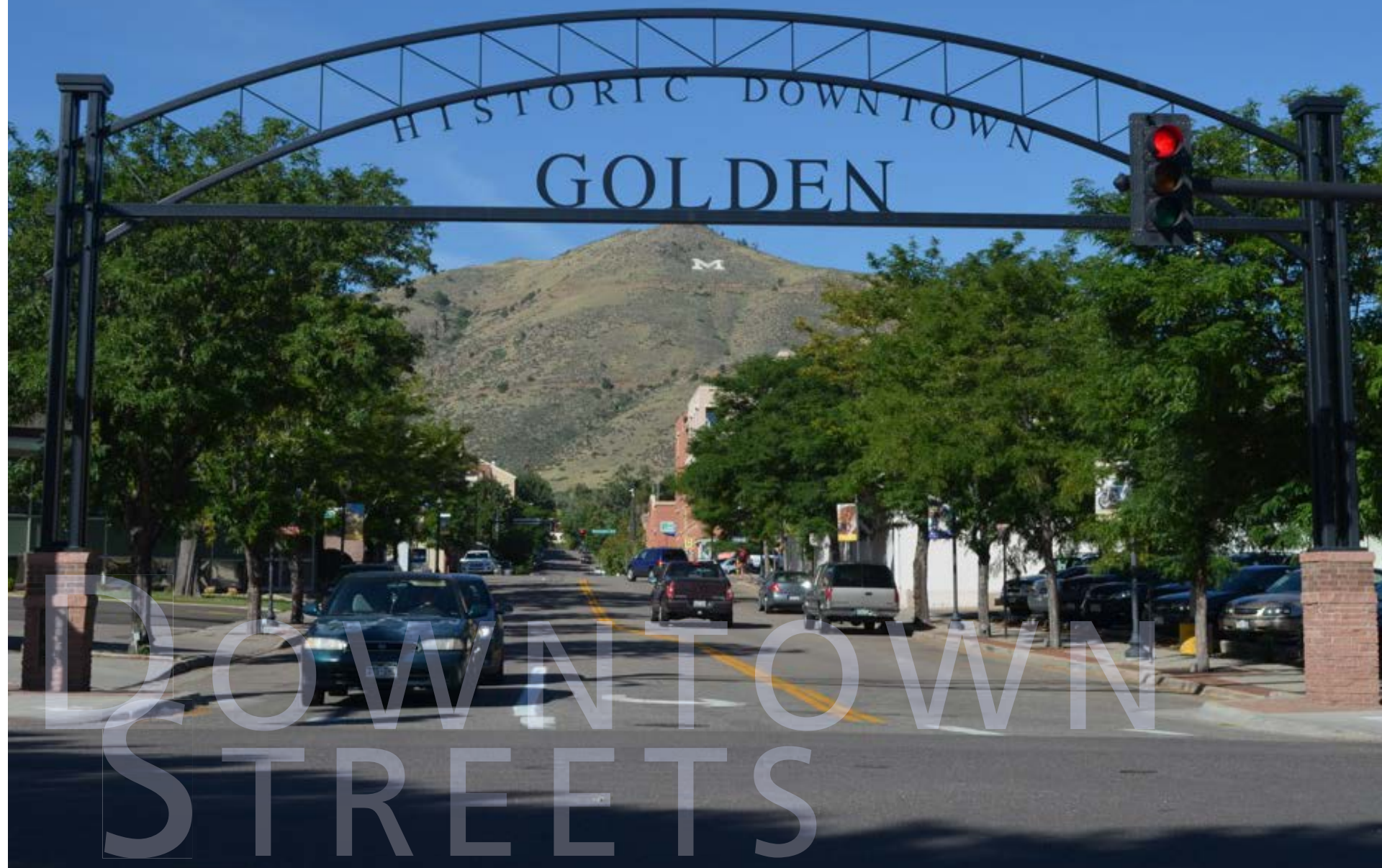
After the charrette, the streetscape design options for each segment of the study area were refined to reflect the stakeholders’ and City Staff’s comments and suggestions. Alternative design options for each area were developed and these alternatives were then presented to the community.



Figure 20 - A 3D Visualization that explores a streetscape option based on feedback from the initial public outreach



# CONCEPTS FOR



## Chapter 4

### Concepts for Downtown Streets





The steps Golden took a little over 20 years ago to revitalize its downtown were bold and an expression of the value the community places on having an active, prosperous city center.

These same values underpin the thinking behind the concepts developed as a part of this Streetscape Master Plan. The concepts that follow explored a wide range of options.

Some are simple and focus on examining ways to work within the existing streetscape framework. Others are more aggressive and look at more fundamental changes, especially for Washington Avenue.

Each idea described and illustrated in this section has the potential to improve the downtown's streetscape. The process of vetting each option to determine its appropriateness for downtown Golden should be an on-going process involving Golden's residents, the downtown community, the decision-makers, and City and GURA staffs.



Figure 21 - A shared street creates a flexible space that can be adapted to a wide range of uses



CONCEPTS WASHINGTON AVENUE -  
CLEAR CREEK TO 14<sup>TH</sup> STREET

Four different concepts were completed for Washington Avenue from Clear Creek to 14<sup>th</sup> Street.

Concept A

In this concept, minimal changes were proposed for basic infrastructure of Washington Avenue. For example, the gutter location and width of sidewalks could remain the same to lessen costs, or when curb replacement is necessary, some reconstruction can occur.

- Concept A creates “Flex” spaces in Washington Avenue (Figure 22). In the Flex Space concept, a portion of the street is designed so that it can be used for a variety of uses. In this case, the parallel parking is enhanced so that it can be used for bike parking, public seating or gathering, or outdoor retail / dining. In order to create the Flex Space:
  - The two travel lanes and the center turn lane are re-striped and reduced to 10’ each. This has the added benefit of slowing traffic.
  - The 4’ to 5’ from the reduced street width is added to the parallel parking areas on the east side of the street to create more opportunities for streetscape amenities.
  - The pavement in the Flex area is enhanced with decorative paving. This sets the area apart from the asphalt travel lanes.
  - Parallel parking is defined by a change in pavement color and/or removable bollards.
  - Outdoor seating and dining areas are shown in the some of the Flex areas. These could be created by eliminating one or two parking spaces and defining / protecting the new gathering area with heavy, narrow portable planters.
- The concept illustrates the idea of removing a portion of a corner planter (lower left in Figure 22) to create an outdoor seating / gathering space.
- The concept also illustrates how the canopy posts create a barrier to pedestrian flow for the sidewalks (lower left of Figure 22).
- This is the least expensive option of the four concepts. Because Washington Avenue is scheduled to be resurfaced, the cost of upgrading the pavement in the parallel parking areas from asphalt to pavers is reduced, and the cost of the new asphalt can go towards reducing the cost of brick or concrete pavers. If it is determined that streetscape improvements are desirable for Washington Avenue, selecting a lower cost option would allow more resources to go elsewhere in the downtown.

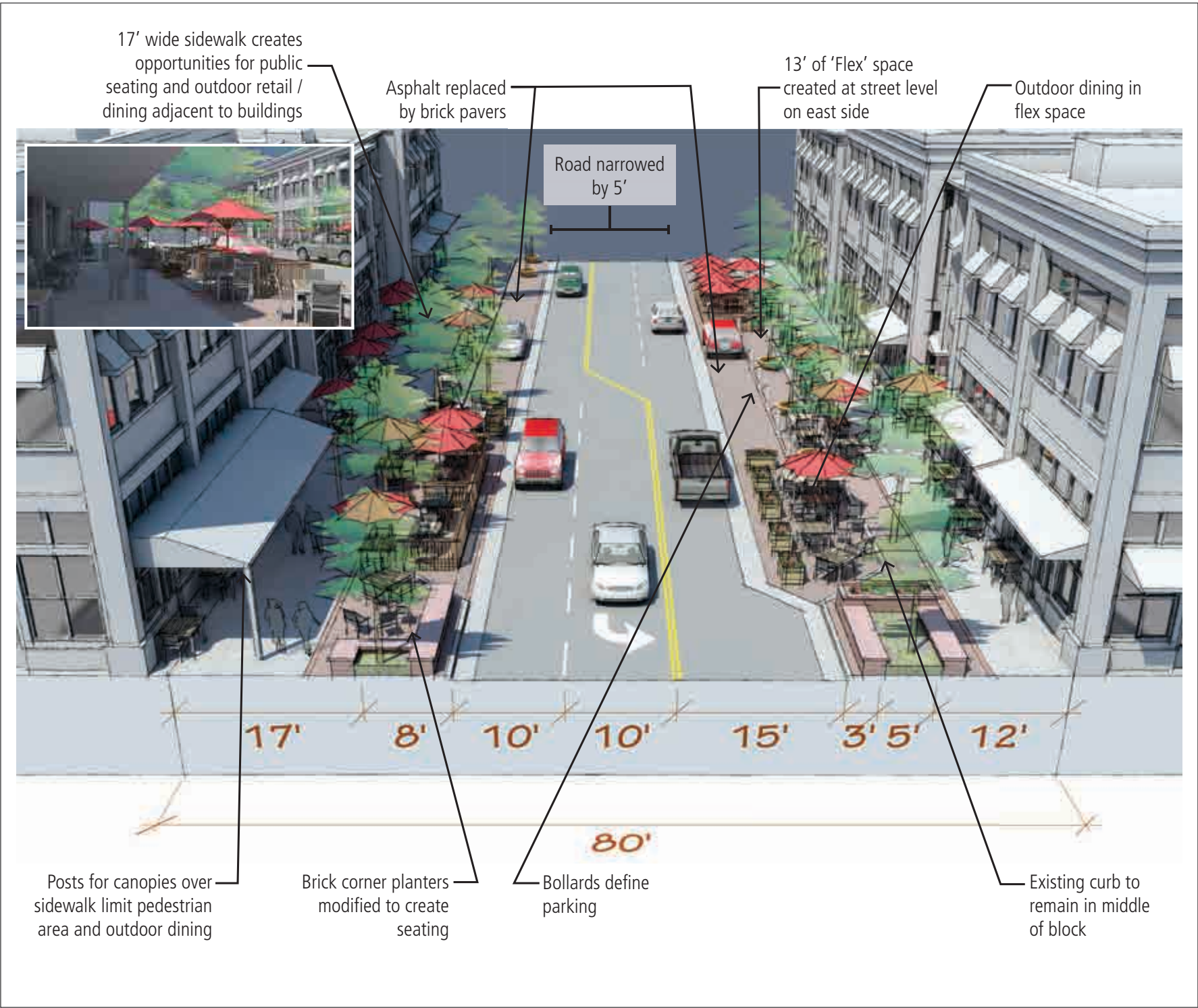


Figure 22 - Washington Avenue Concept A (looking north)



Concept B

Concept B creates a symmetrical street section by reconstructing the east curb line 4' to 5' to the west. This creates a 17' wide sidewalk on the east side of the street matching the wider walk on the west side.

- ↪ The east curb line is moved toward the center of the street to create two 10' travel lanes and a 10' center turn lane.
- ↪ A +/- 17' sidewalk on the east side of the street creates opportunities for public seating and outdoor dining adjacent the building facades. This is very difficult to achieve with the existing 12' wide sidewalk.
- ↪ As with Concept A, a Flex Space is created in the parallel parking spaces and decorative pavers added. This maintains the option of removing one or two parking spaces to create bike corrals, public seating areas, or outdoor dining.
  - It may also be possible to raise the street to the sidewalk level with temporary pavers (set over the existing asphalt), where there is no conflict with storm drainage.
- ↪ It will be necessary to complete detailed engineering studies to determine if these changes can be accomplished given the slope of the street.
- ↪ The concept illustrates the idea of removing a portion of a corner planter (lower left of Figure 23) to create an outdoor seating / gathering space.



Figure 24 - Flex space used for outdoor dining

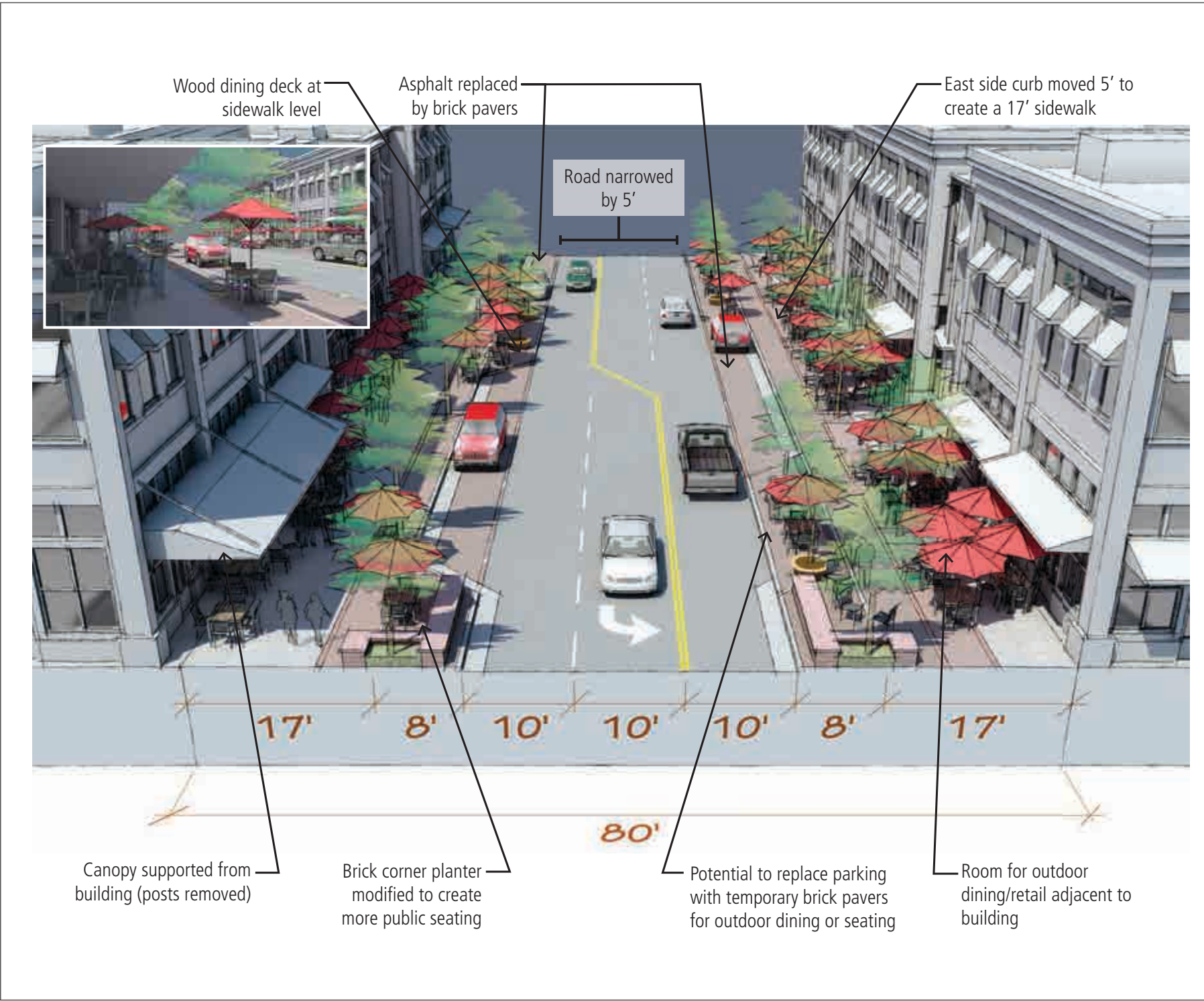


Figure 23 - Washington Avenue Concept B (looking north)



Concept C

The design approach for Concept C (Figure 25) is to reduce the travel lanes for Washington Avenue, add curves to slow traffic, and create larger spaces for streetscape amenities. This concept:

- ↳ Creates large areas on the inside of each curve that can be used for outdoor seating and gathering spaces, outdoor dining, or activity generating amenities such as a pop-jet water feature or children’s play sculpture.
- ↳ Increases the width of the sidewalk on the east side of the street by 5’ so that it matches the width of the west side. This creates additional sidewalk area and opportunities for public seating and outdoor retail / dining.
- ↳ Requires the reconstruction curb and gutter in the new locations and adjusting the locations of storm drain inlets. It will be necessary to complete detailed engineering studies to determine if these changes can be accomplished given the slope of the street.
- ↳ Eliminates the sidewalk canopy posts and extends the canopies from the building façade (lower left of figure 25).
- ↳ The curve in the street eliminates the parallel parking on one side of the street, but replacing parallel parking with angle parking on the opposite side of the street will retain 65% of the initial on-street parking spaces. This concept shows back-in angle parking (see below).

What is back-in angle parking?

This type of parking is designed for drivers to back into the parking space rather than pull into the stall. The angle of the parking space is reversed from what is seen with the head-in style and allows for more parking stalls per block than parallel parking does. To enter the stall, the driver would slow down and then signal just as one would do for parallel parking before backing into the space.



Back-in angle parking provides a safer environment for bicyclists since drivers are able to see them easier and sooner when exiting the parking stall. It also allows people in standard size or smaller vehicles see around large SUVs and trucks entering the travel lane. And, back-in angle parking provides a safer environment for people to load items into the trunk since the trunk of the vehicle is adjacent to the sidewalk and not to the travel lanes.

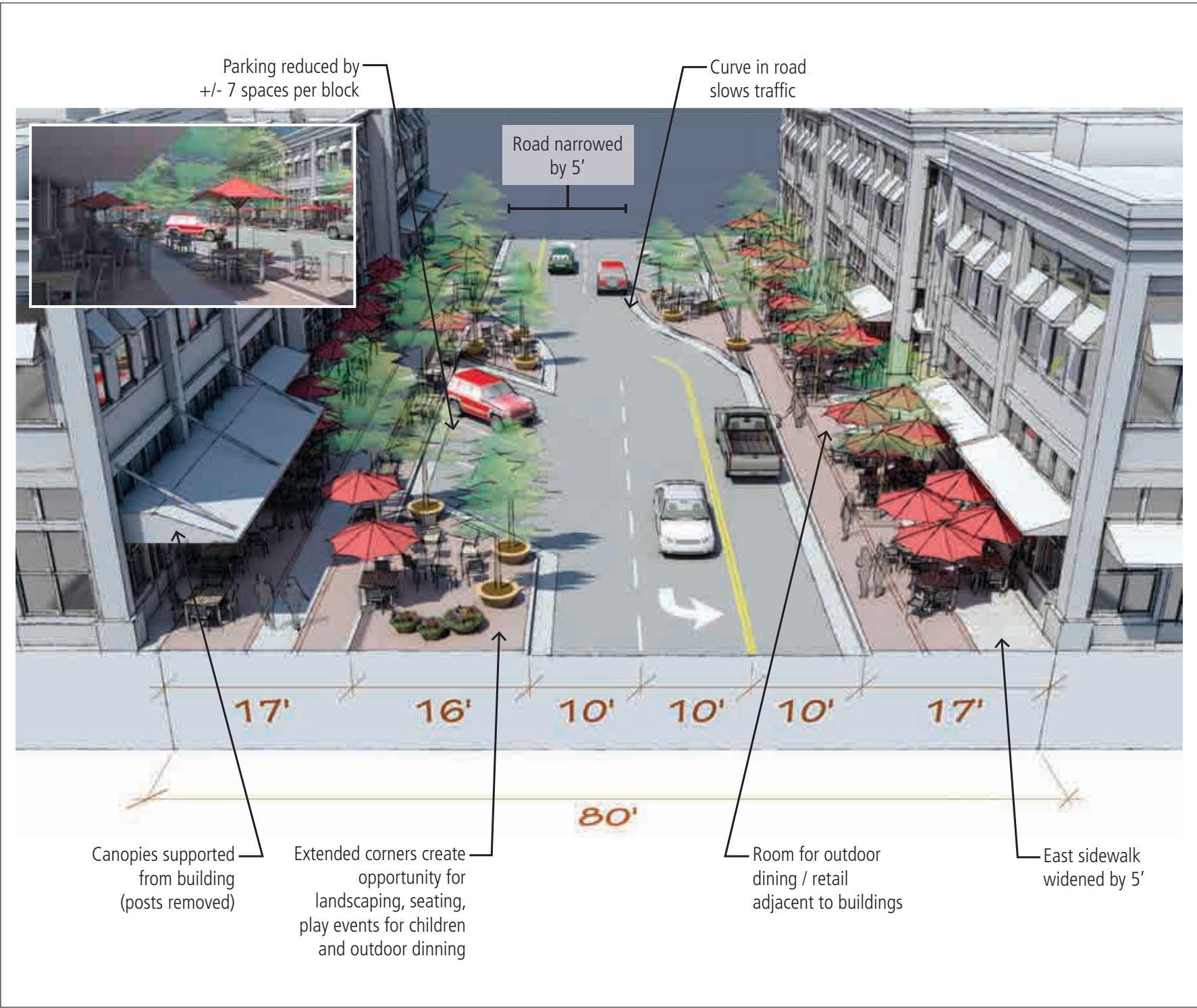


Figure 25 - Washington Avenue Concept C (looking north)



Concept D

The “Shared Street” concept is illustrated in Concept D. In this approach, the street is raised to the level of the sidewalks and curb and gutter eliminated. This provides the highest level of flexibility for the streetscape, allowing it to be used for a wide range of uses.

- The Shared Street does not close Washington Avenue to vehicles. The travel lanes are defined by changes in paving texture and color, flush concrete bands or drainage pans, and bollards.
- Because there are no curbs, a 25’ streetscape zone is created on each side of the street. This area can be configured in any number of combinations:
  - With or without parallel parking.
  - Benches or movable chairs can be added in groups, providing places for people to congregate.
  - Streetscape amenities such as water features or children’s play sculptures can be added.
  - An additional row of street trees or flowering ornamental trees can be planted and decorative pots for annuals added. The existing brick corner planter could be retained, modified, or eliminated.
  - Festivals can be more easily accommodated and enjoyed by patrons as there are no curbs to trip over or limit vendor tent locations.
- Because of Golden’s unique topography, this concept would need to be carefully engineered. Detailed engineering may show storm flows cannot be accommodated or they could show that some combination inlets, trench drains, and porous paving can safely accommodate storm flows.
- This is the most costly of the four concepts presented.



Figure 26 - Shared street example (urban greenway)

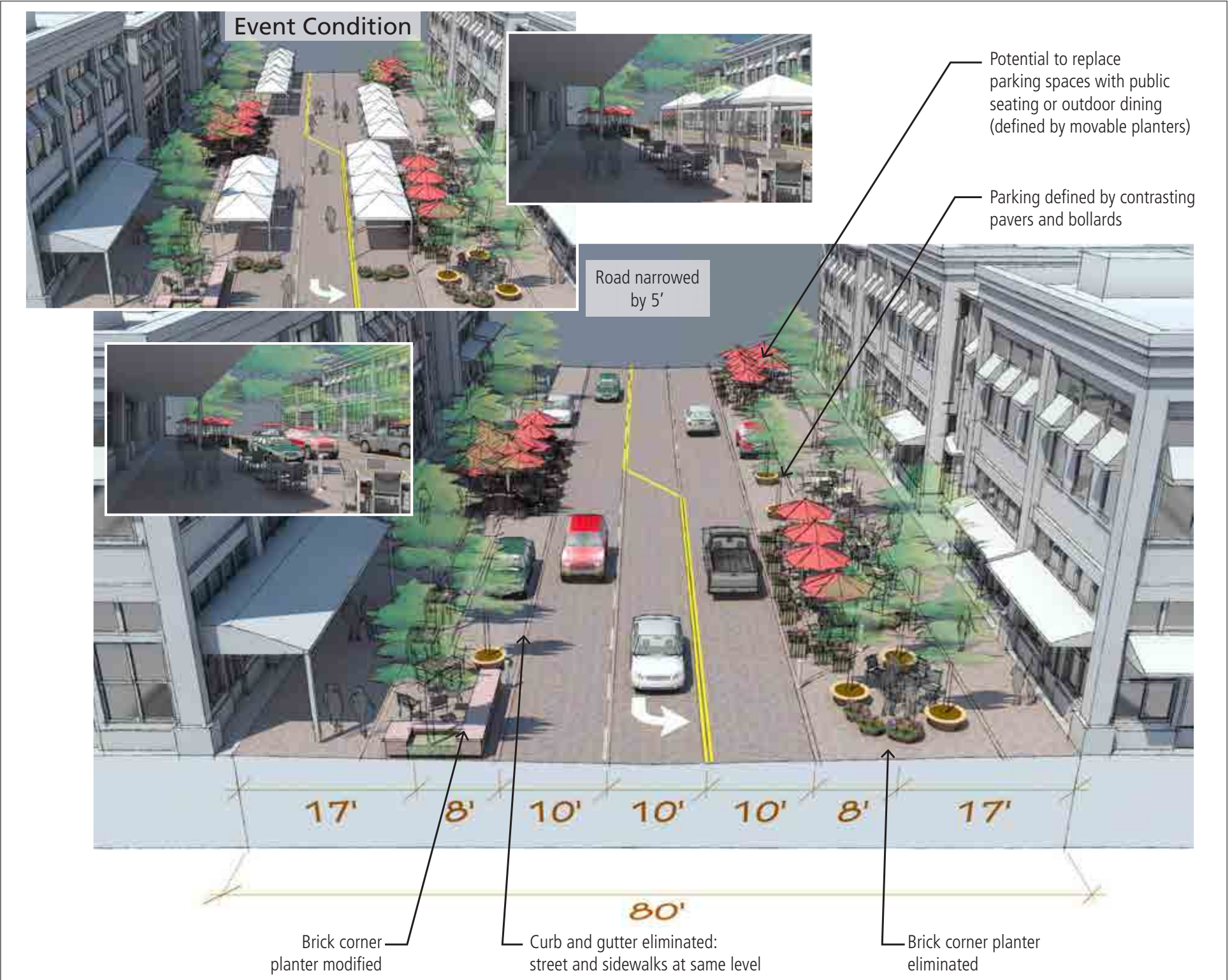


Figure 27 - Washington Avenue Concept D (looking north)



OPTIONS FOR WASHINGTON AVENUE’S BRICK/STONE CORNER PLANTERS

During the public engagement process, stakeholders from the downtown community were asked in a questionnaire to rank potential streetscape improvements. The highest ranking improvement was to “remove a wall of a brick corner planter(s), the plants and soil, and replace with seating”. Based on this input, three options for revising the corner planters were developed (Figure 29).

- **Option A:** Retain the street side wall of the planter, one half of the planting bed, a tree, and the end cap closest to the corner. This created an opportunity for a small seating / outdoor dining area (three to four tables).
- **Option B:** Retain the street side wall of the planter and the end cap closest to the corner. This created a much larger area that could be used for seating, outdoor dining (five to six tables), or a small children’s climbing sculpture surrounded by benches.
- **Option C:** Totally eliminate the corner planter and replace it with a bosque of flowering ornamental trees and wide range of streetscape amenities (six to eight outdoor dining tables shown).



Figure 28 - Not every corner in downtown has a brick planter



Figure 29 - Different configurations for revising Washington Avenue’s corner planters were presented



WASHINGTON AVENUE 14<sup>TH</sup> STREET TO 18<sup>TH</sup> STREET

The concept plan for Washington Avenue south of 14<sup>th</sup> Street (Figure 30) illustrates a group of enhancements that would improve pedestrian circulation and increase parking.

- In order to reduce the steep grade of the sidewalk on the west side of the street between 14<sup>th</sup> and 15<sup>th</sup> Streets, an additional wide sidewalk has been placed at street level, thereby reducing the slope as much as possible. Once past 15<sup>th</sup> Street, the sidewalk route is returned to its original alignment.

- Back-in angle parking is shown on the west side of the street between 15<sup>th</sup> and 18<sup>th</sup> Streets. This increases available parking by about double (as compared to parallel parking) and is safer than head-in angle parking.
- In order to construct these two enhancements, the traffic lane configuration would need to be changed to one lane in each direction, which is feasible given the traffic volume for this segment of Washington Avenue.
- Re-landscaping the steep embankment between 14<sup>th</sup> and 15<sup>th</sup> Streets and planting additional street trees throughout the corridor is also proposed.

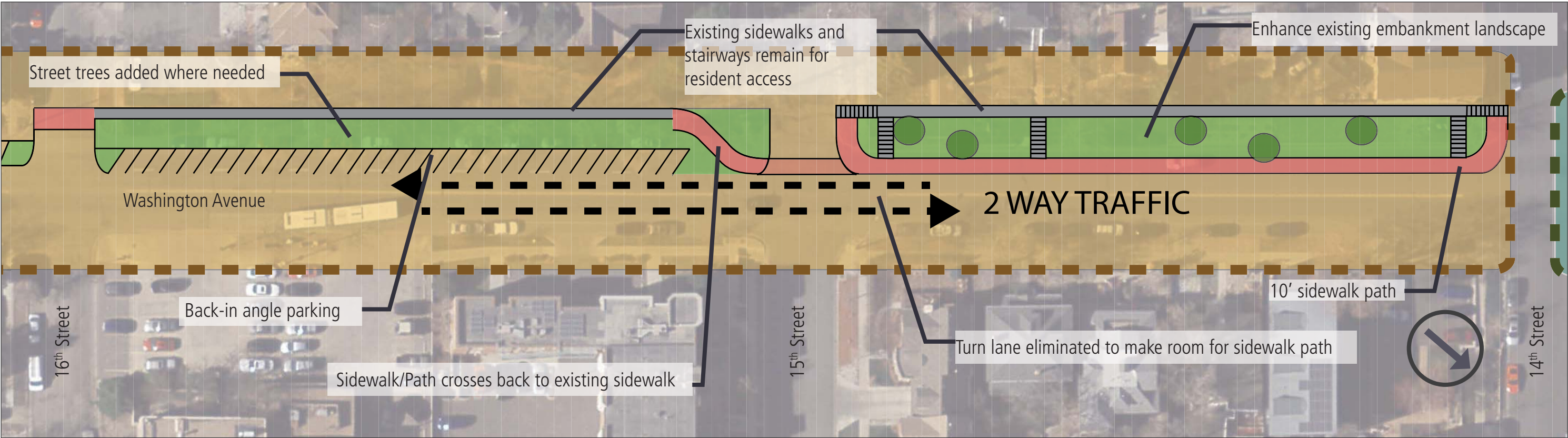


Figure 30 - An idea for improving pedestrian access on Washington Avenue south of 14<sup>th</sup> Street



JACKSON AND 13<sup>TH</sup> STREET FESTIVAL AREA CONCEPT

There is a limit to the number of festivals that can be held on Washington Avenue because of the impact on downtown traffic flow and the costs associated with the street closure. Because of this, it is difficult to approve requests for additional large events or to accommodate smaller / start-up festivals. Therefore, the concept of creating a second festival area in the downtown has been examined (see Figure 31).

- Two blocks each on Jackson Street and 13<sup>th</sup> Streets are shown as a “plus sign” shaped area for special events. Removable or retractable bollard could be used at the end of each street to allow the closure of from one to four blocks for a festival.
- These streets could be used in conjunction with Washington Avenue for very large special events.
- 13<sup>th</sup> Street, especially the western block, appears well suited for this use because it is visible from Washington Avenue and the MillerCoors brewery tour parking lot.
- It may be necessary to open 14<sup>th</sup> Street to two-way traffic between Washington Avenue and Ford Streets to accommodate rerouted traffic during a festival in this area.

Removable bollards are approximately \$1,000 each and manually retractable bollards from \$2,000 to \$4,000 each.

The number of bollards needed varies by intersection, depending on whether there are curb extensions or not.

Assuming removable bollards are needed at the end of each block for the concept shown, approximately 16 bollards would be needed for Jackson Street and 15 bollards for 13<sup>th</sup> Street, totaling approximately \$30,000 to \$35,000. That cost would at least double for manually retractable bollards.

Hydraulically operated retractable bollards have become popular recently, but the high cost (approximately \$50,000 for two) and limited data on long-term performance winter climates where streets are sanded, make this option less attractive.

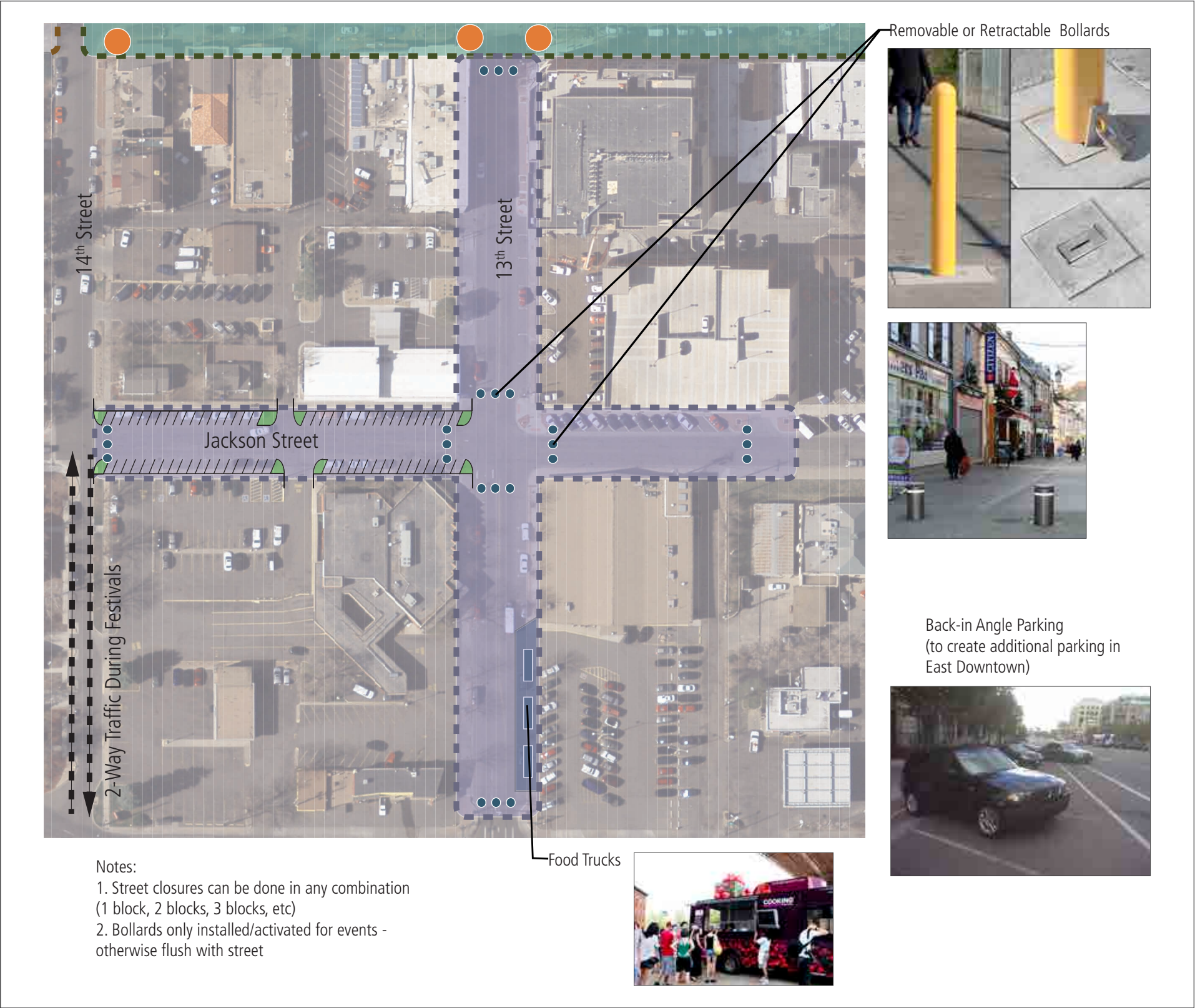


Figure 31 - Jackson Street festival concept presentation board



JACKSON STREET PAVEMENT TO PARK CONCEPT

One option for activating the portion of the study area east of Washington Avenue is to convert an underused section of street to a more active use. The concept to develop one half block of Jackson Street to a “Pavement-to-Park” illustrates this idea.

- In this concept a colorful children’s play structure with rubberized safety surfacing is shown along with play area seating, an opportunity for a food truck and café tables, seating with a fabric shade structure, movable chairs, and active uses such as a basketball hoop or table tennis. Trees and landscaping could be provided in large portable pots or planters. Depending on the scale of the improvements, the cost could be between \$75,000 and \$150,000 and most elements could be moved to another location if the street was re-opened to traffic.

- The street park would be visible to families as they pull into the public parking structure. If done correctly, families with small children will visit Washington Avenue and also spend time in this section of the downtown.
- It would soften the blank west wall of the Wells Fargo Wire Transfer building.
- The area could be designed to accommodate vendor tents during festivals.
- The Pavement-to-Park conversion would be built on top of the existing street leaving it intact and available for conversion back to traffic should adjacent uses or traffic flow change.
- Closing this portion of street would create limited impacts on traffic flow in the downtown. Access to the parking structure would be maintained at all times and traffic from the uses north and south of the block could easily use either Ford Street or Washington Avenue.



Figure 33 - San Francisco pavement to park - Castro Street  
(image courtesy of Pavements to Parks)



Figure 34 - A temporary pavement to park installation  
(image courtesy of the Congress for New Urbanism)

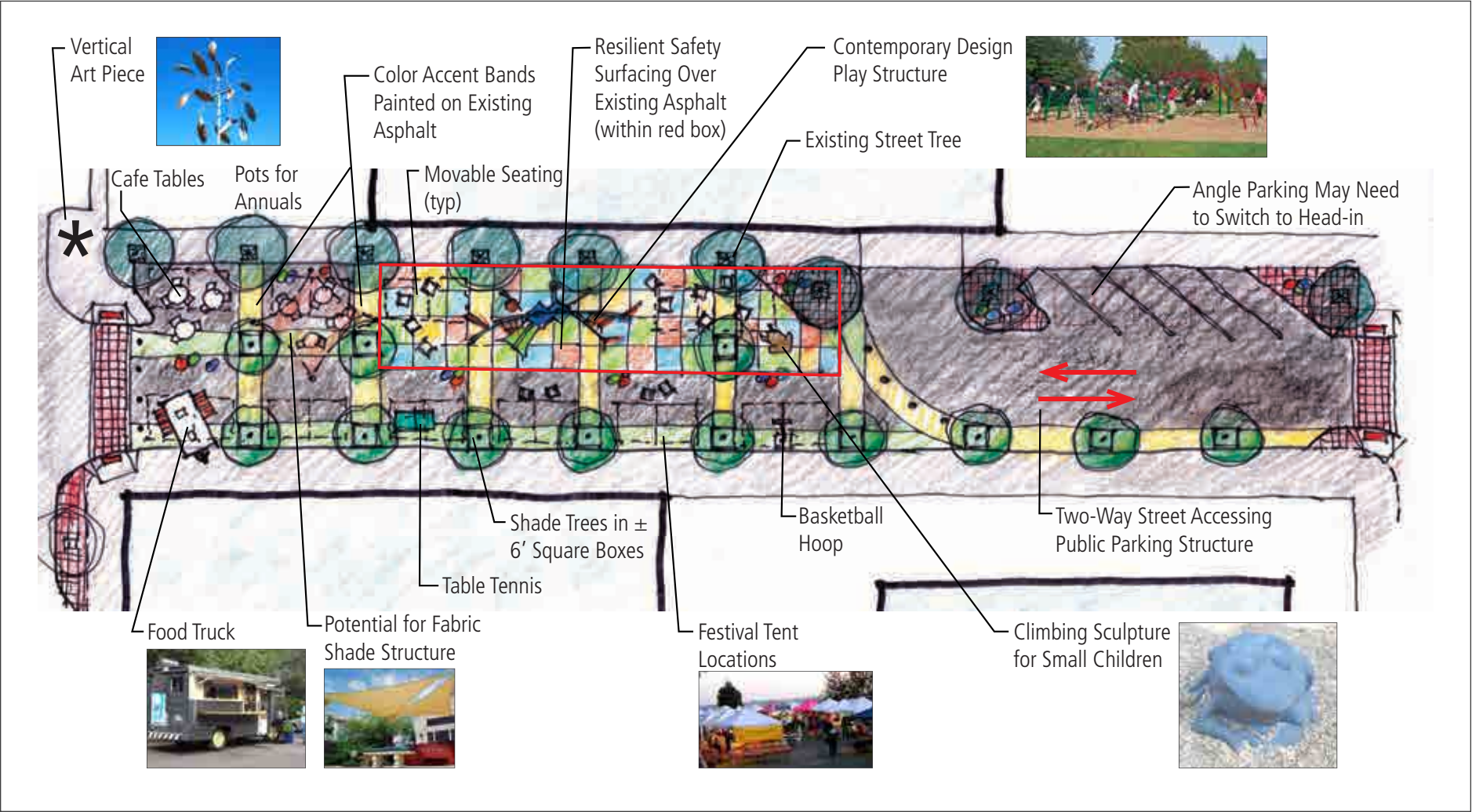


Figure 32 - Jackson Street Pavement to Park concept presentation board







## Chapter 5

### Public Review and Comment







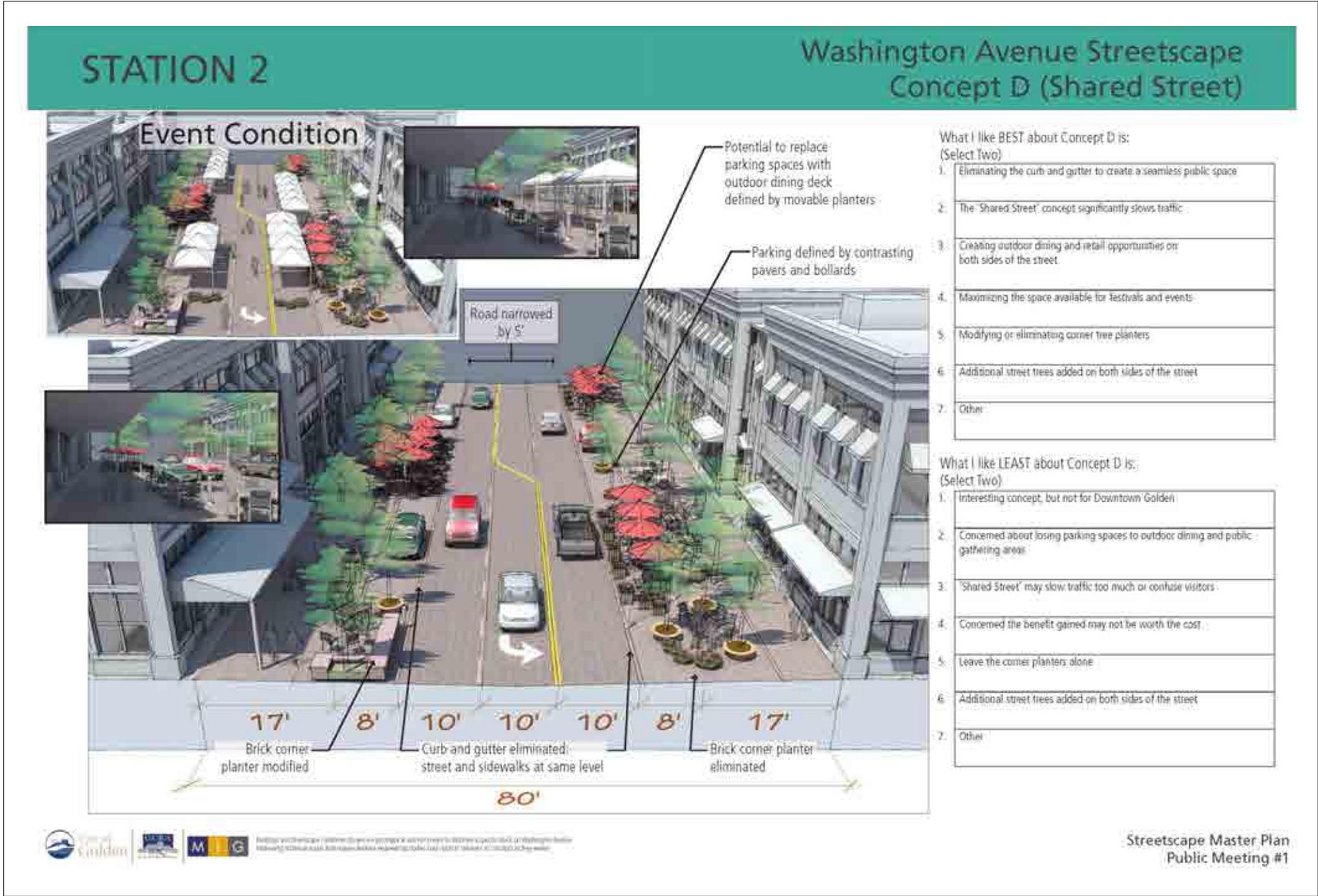


Figure 35 - One of the posters used at the July open house illustrating the shared street concept

In July of 2013, the community had the opportunity to review and comment on the streetscape design options for each segment of the study area. The design alternatives were developed in response to stakeholders' and City Staff's comments and suggestions during the Initial Public Outreach (see Chapter 3). The community was then asked to comment on the concepts via an open house, MindMixer website, and / or emails. The intent was to identify the preferred streetscape enhancements for the study area.

**JULY OPEN HOUSE**

At the open house, a series of stations were set up to display the streetscape concepts.

- Station 1 focused on identifying what participants liked best and like least about the existing Downtown. (The results of this exercise can be found in Chapter 1).
- Station 2 displayed four concepts for Washington Avenue.
- Station 3 displayed brick planter options for Washington Avenue.
- Station 4 focused on the Washington Avenue streetscape south of 14<sup>th</sup> Street.
- Station 5 presented the concepts for a festival area on Jackson and 13<sup>th</sup> Streets.
- Station 6 displayed a pavement-to-parks concept for Jackson Street.



**Station 2 - Washington Avenue Streetscape Concepts A through D**

Concepts A, B, C, and D for Washington Avenue (Clear Creek to 14<sup>th</sup> Street) studied a range of options for improving Washington Avenue’s streetscape experience. See Chapter 4 for detailed descriptions of each of the concepts listed below.

*Concept A*

Participants at the open house indicated that the aspects they like best about Concept A were adding wood or brick paver decks in the Flex space to create outdoor dining at the sidewalk level and also adding brick paving in the parallel parking spaces to reduce asphalt. Participants indicated that the aspects they liked least were the mixing of parking and outdoor dining and making changes to the existing corner planters. A few participants indicated that reducing on street parking was a concern.

*Concept B*

The features of Concept B that open house participants liked best were increasing the options for outdoor dining and retail, adding brick pavers in parallel parking areas, and extending the curb and gutter along the east side of the street to increase the sidewalk width to 17’. Aspects liked least included mixing parking stalls and outdoor dining and making the road too narrow.

*Concept C*

There was less distinction between the features liked best and least for Concept C. Participants identified the aspects of Concept C that they like best as the mid-block crossing, the wide sidewalks, the removal of posts that support the sidewalk canopies, and the creation of outdoor dining on both sides of the street. Aspects liked least were the negative impact on traffic flow and that concept of curving Washington Avenue to create more people spaces was not appropriate for Downtown Golden.

*Concept D*

Participants indicated that the elements of Concept D that they liked most were the creation of equal outdoor dining on both sides of the street, the maximization of festival space, and the addition of street trees. Participants thought that the concept might too aggressive in slowing traffic and may be confusing for visitors.

**Station 3 – Washington Avenue Brick Planter Options**

At Station 3, participants provided feedback to four alternatives for the brick corner planters. The alternatives that received the most support were:

- 1) leave the planters as is.
- 2) remove the entire planter and add ornamental trees and decorative movable planters around the seating area.

**Station 4 – Washington Avenue South of 14<sup>th</sup> Street**

At Station 4, participants provided feedback on streetscape options for Washington Avenue on the blocks south of 14<sup>th</sup> Street. The improvements that received the most responses were creating the path at street level to provide a more pedestrian-friendly route, encouraging people to visit the south half of Downtown, and converting the parallel parking to back-in angle parking to increase the number of spaces. However, there were a number of respondents who expressed concern about the back-in angle parking. The need for a crosswalk at 14<sup>th</sup> Street was also identified by participants, although it was not originally an option from which to choose.

**Station 5 – Jackson and 13<sup>th</sup> Street Festival Area**

At Station 5, participants provided feedback on the idea of creating a festival area on four blocks of Jackson and 13<sup>th</sup> Streets. Participants indicated that the features liked best were the potential of encouraging redevelopment of adjacent properties, the creation of a large area for festivals off of Washington Avenue, and that the area could be used in combination with Washington Avenue to host large events. The aspect that some participants liked least was that this new festival area might take events off of Washington Avenue.

**Station 6 – Jackson Street Pavement to Park Concept**

Station 6 presented the details of closing a segment of Jackson Street north of 13<sup>th</sup> Street to implement a “Pavement-to-park”. In this concept, a half-block segment of the street would be closed to vehicles and populated with activity generators such as a children’s play area. Participants indicated that they liked the concept of having a small festival space off of Washington Avenue for start-up events that do not justify closing Washington Avenue, the creation of a destination that would attract people from Washington Avenue down 12<sup>th</sup> and 13<sup>th</sup> Streets, and that the area provided play opportunities Downtown. Food trucks being competition to local restaurants and the impact of closing Jackson Street on traffic flow in the Downtown were found to be concerns.

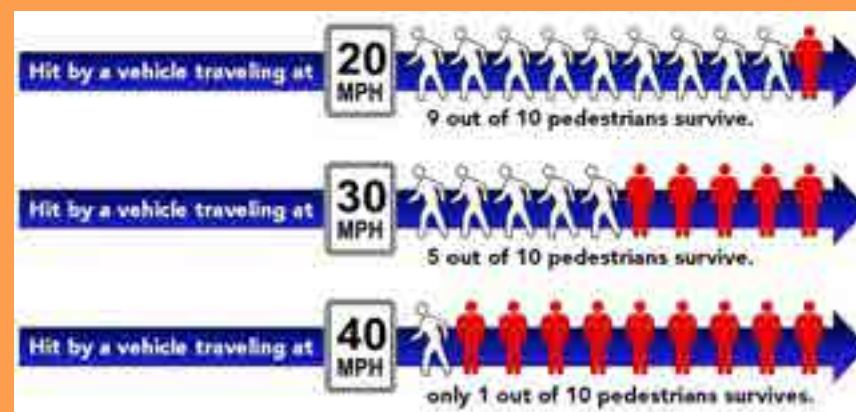


Figure 36 - Moving east side curb out five feet creates room for outdoor dining at buildings



Figure 37 - Brick corner planters do provide limited opportunities for seating, but limit other uses





Higher speeds mean more fatalities in pedestrian accidents. Narrowing Washington Avenue (favored by public comments) would reduce the speed of vehicles traveling it and increase pedestrian safety.



Figure 38 - MindMixer feedback noted that dining decks too close to traffic were a concern. However, Louisville's (shown above) are very popular.

## MINDMIXER REVIEW

The concepts presented at the open house were also posted on MindMixer to expand the level of participation and gather additional feedback from those unable to attend the open house. The responses to the concepts presented at the open house were slightly different those received on MindMixer. This may have to do with the limits on the number of questions that could be posted on the MindMixer, which resulted in some open house questions being combined.

Washington Avenue Concept A, MindMixer participants indicated that in addition to adding dining decks in the Flex space area, they also highly favored narrowing the street to slow traffic. They did not support mixing the parking with outdoor dining. There were more responses to leave the corner planters alone and concern that the concept did not create outdoor dining immediately adjacent to buildings.

For Washington Avenue Concept B, they liked the removal of posts that supported the sidewalk canopies, extending the curb to widen the sidewalk to 17' on the east side, and adding street trees on the east side – both top answers not seen at the open house. Participants indicated aspects that they liked least were the mixing of parking and outdoor dining and making changes to the corner planters.

For Washington Avenue Concept C, participants – like those at the open house – liked the mid-block crossing and the wide sidewalks best. They were concerned about the back-in angle parking and indicated a dislike of any modifications being made to the corner planters.

For Washington Avenue Concept D, participants indicated the best improvements were additional street trees on both sides of the street and removing curb and gutter to create a seamless public space. They did not like the outdoor seating being close to traffic or any modifications made to the corner planters.

In response to Washington Avenue Planter Options (Station 4 at the open house), the MindMixer participants re-emphasized the opinion to leave the corner planters alone or to only remove half of each planter to create space for outdoor seating.

In regard to the concepts for Washington Avenue South of 14<sup>th</sup> Street (Station 5 at the open house), MindMixer participants liked the idea of enhancing the landscape for the steep embankment on the west side of the road between 14<sup>th</sup> and 15<sup>th</sup> Streets and the street-level path to encourage visitors to explore the south half of Downtown. The aspect that they liked least was the back-in angle parking.

The response for the Jackson Street Pavement-to-Park concept reflected the results of the open house. The aspect respondents liked best was the creation of a small festival space for start-up events that do not need all of Washington Avenue and the aspect respondents liked least was the closure of Jackson Street to thru-traffic.

Features liked the best in regard to the Jackson & 13<sup>th</sup> Streets Festival Area included the creation of a festival area beyond Washington Avenue and the flexibility to close one, two, three, or four blocks at a time for street events. Once again, participants indicated less desire for the back-in angled parking.

## EMAIL COMMENTS

Another method of gathering public feedback on the concepts presented at the open house was to invite interested parties to email comments. Themes from the emails included:

- Concerns about further restricting vehicular traffic and increasing congestion in the Downtown.
- Concerns about blocking access to/from businesses and residences located at the north end of Jackson Street if a portion of that road were to be closed to vehicles.
- Direction that designs for the corners should accommodate right-turn lanes and not restrict visibility.
- The need for better general maintenance of trash receptacles.
- The observation that outdoor dining areas can make it difficult to walk down the street and that loud behavior, panhandling, and/or smoking made the sidewalks unappealing.
- There was a request for more information on how streets without curb and gutter would handle storm water.

SUMMARY OF PUBLIC REVIEW AND COMMENTS

When reducing the information gathered down to basic direction, people who commented at the open house and via the MindMixer format generally supported the concepts of:

- ↳ Flex space decks and/or paving – particularly for outdoor dining.
- ↳ Wider, unobstructed sidewalks.
- ↳ A mid-block crosswalk.
- ↳ A play area in the Downtown.

However, they:

- ↳ Tended not to like the idea of mix parking and outdoor dining areas.
- ↳ Were uncomfortable with back-in angled parking.
- ↳ Expressed concern about traffic impacts and loss of parking due to the various design alternatives.

In both groups, there was a mixed reaction toward whether or not alterations should be made to the existing corner planters.

There was also a split in attitude for closing Jackson Street or 13<sup>th</sup> Streets to create a new festival area. The majority felt it would be beneficial to create an area for festivals off Washington Avenue; but, for some, closing the street raised traffic concerns, for others, that it would create competition with Washington Avenue events and businesses.

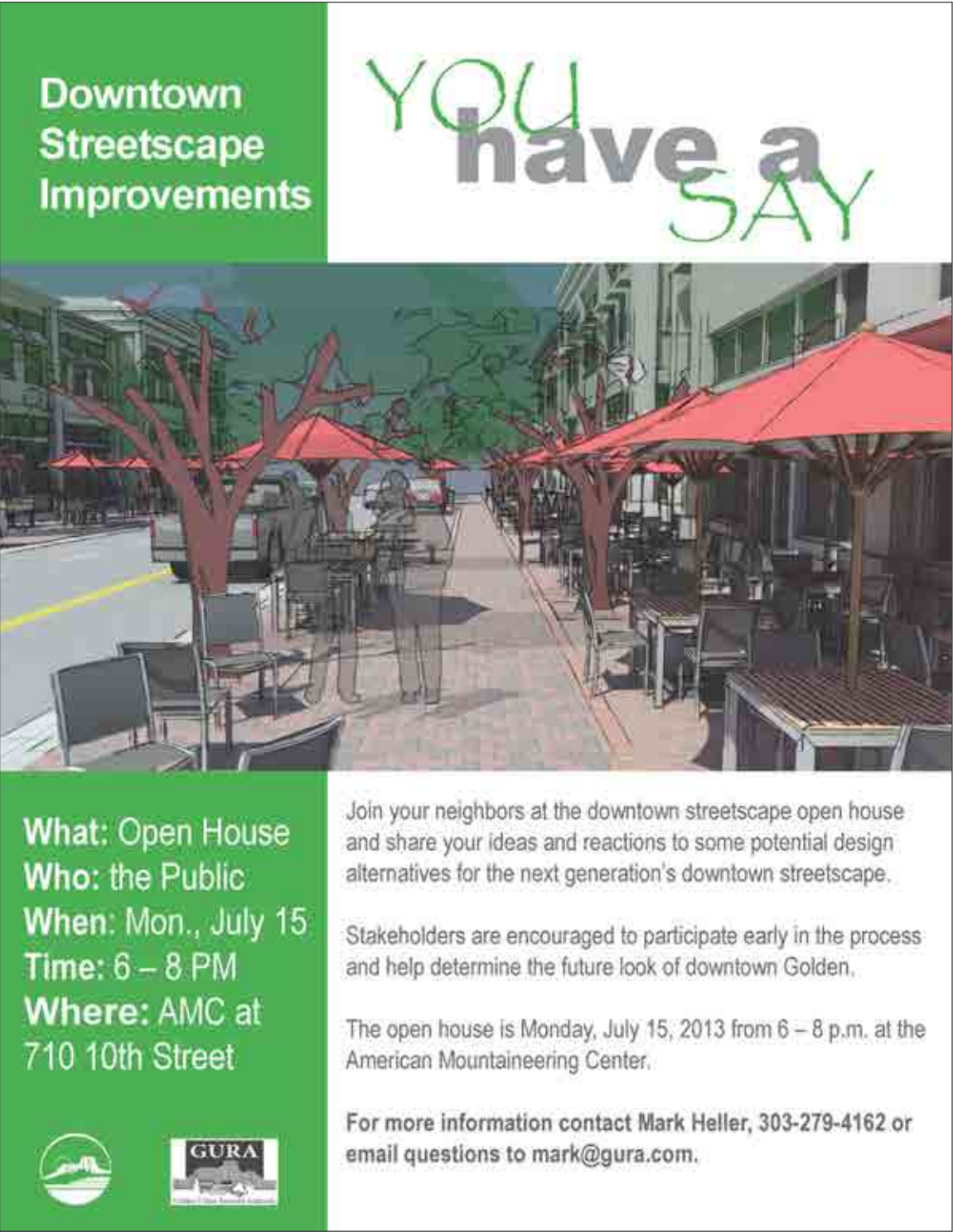


Figure 39 - Flyer distributed for open house





## Chapter 6

### Preliminary Recommendations





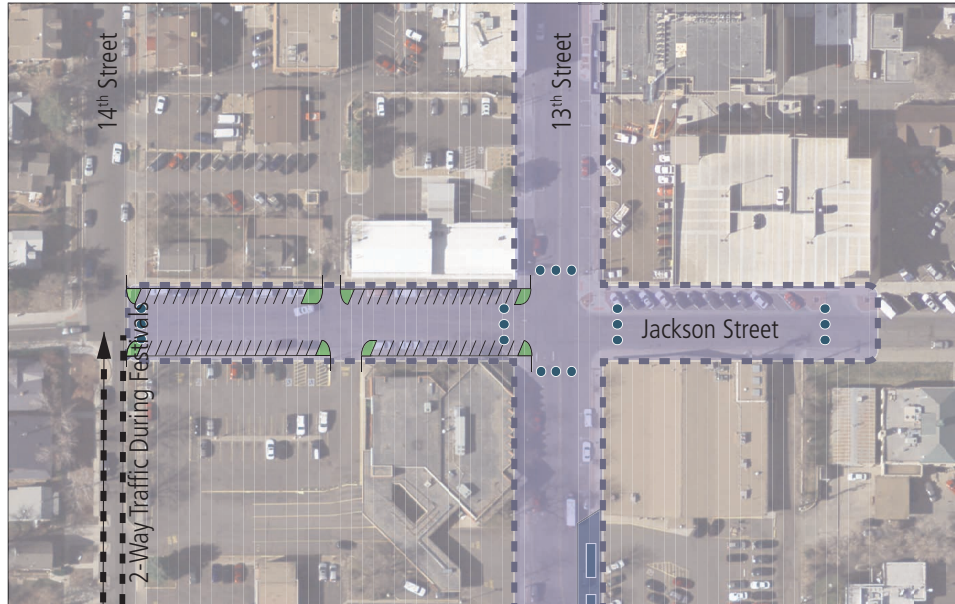


Figure 40 - Jackson Street festival area concept

Golden is exceptional in many ways. Its geographic context is unique, even for Colorado. The city has a strong economic base and a diverse mix of employers, which means residents can live and work in the same community. It has a healthy retail environment both in and outside the downtown, and also has an established set of values and clear vision of what being “Golden” means.

The streetscape in downtown Golden is a sum of all of these parts and, because of hard work and ongoing investment, is not in need a complete facelift. That being said, the community leaders don’t want to rest on their laurels. There is a desire to consider new ideas and make bold changes when necessary.



Figure 41 - The Chili Cookoff Festival attracts large crowds to the downtown

The following recommendations are presented with this foundation in mind. They are not mandates, but rather ideas that may in some cases need time to gestate before being adopted. Each of the enhancements listed below is targeted toward making Golden’s urban environment more active; more comfortable for patrons, and with more flexibility so that businesses of all types can be successful. The recommendations are generally listed in order of relative cost/benefit, perceived need, and favorable rankings during the public engagement process. However, the desirability of a particular recommendation may change as the downtown changes. Each improvement implemented in the future should be evaluated against the needs of the downtown at that time.

### JACKSON AND 13<sup>TH</sup> STREET FESTIVAL AREA

Establishing a festival area for four blocks of the East Downtown is a “quick win”. The cost of creating a festival area using the two blocks of 13<sup>th</sup> Street (between Washington Avenue and Ford Street) coupled with one and one-half blocks of Jackson Street (14<sup>th</sup> Street to the entry to the parking garage) is relatively low and would provide the most flexibility.

While this might not result in an immediate payoff, increasing the number of special events and festivals in the downtown without impacting Washington Avenue traffic flow should bring more people downtown and encourage development of adjacent properties.

- ↪ If removable or retractable bollards are installed at the end of each block, the configuration of the street closures could vary to accommodate different size festivals.
- ↪ Maintenance staff would need to install / remove or raise / lower the bollards for each festival to close the blocks needed for that particular event.
- ↪ If not already in place, it will be necessary to enhance the electrical service for the festival area by adding several electrical receptacles in each block, perhaps at each light fixture.
- ↪ Depending on the configuration of the street closure, it may be necessary to allow two-way traffic on 14<sup>th</sup> Street between Jackson and Ford Streets.
- ↪ Adding food trucks along 13<sup>th</sup> Street is shown, but not integral to implementing this concept. The Jackson Street pavement-to-park concept would enhance the festival area, but once again is not essential to its implementation.
- ↪ There was strong support for the Jackson & 13<sup>th</sup> Street festival area during the open house and Mindmixer review. The downtown stakeholders were less supportive during the focus groups.



WASHINGTON AVENUE 14<sup>TH</sup> STREET TO 18<sup>TH</sup> STREET

Improving the streetscape and pedestrian experience for Washington Avenue south of 14<sup>th</sup> Street is another relatively low cost improvement for the study area.

- ↳ It's possible to create a pedestrian route in the first block that is not as steep as the existing west side sidewalk. This can be accomplished relatively inexpensively by restriping traffic lanes and adding new curb and gutter and a wide sidewalk at the street grade on the west side of the street.
- ↳ Filling in the gaps in the street tree canopy for the remainder of the street will help maintain the character of the existing mature street tree canopy over time.
- ↳ Back-in angle parking can be added to help meet the demand for business and CSM student / faculty parking. This would also be a good opportunity implement back-in angle parking as a test case for the downtown or other areas of Golden.
- ↳ This concept was well received during the public engagement process with the majority of the concern focused on the back-in angle parking.

SITE AMENITIES

An effort should be made to address the shortfall of benches, bike racks, and other site furnishings in the downtown. Installation of a trash receptacle system that also provides opportunities for recycling would be a valuable addition.

- ↳ A single system of site furnishings should be adopted (or a set of site furnishings if diversity is preferred).
- ↳ An inventory should be completed to identify where site furnishing could be added within the existing structure of the downtown. Site furnishings should be installed where space allows and could be installed as a "Flex" space test case by eliminating a parking space or two in each block.
- ↳ More bike parking should be added in each block. This could be another opportunity to test the Flex space approach by eliminating a parking space where bike demand appears to be the highest (in front of the bike store at 13<sup>th</sup> and Washington for example).

- ↳ The City should identify the best approach for pairing recycling receptacles with trash receptacles.
  - Due to capacity limitations and difficult maintenance, the single trash receptacles in the brick corner planters should be abandoned.
  - A combination trash/recycling system should be selected as a downtown standard. There are a wide range of systems with double (trash / recycling) and triple (trash, bottles & cans, and paper) receptacles available. The compacting receptacles described at the end of Section 2 should be tested either on a trial basis or as a purchase to see if they are durable and if they reduce maintenance requirements. Because of their size, locations for combined trash/recycling systems will need to be identified as part of a full evaluation for upgrading the site furnishings in the downtown.
- ↳ During the focus groups, downtown stakeholders noted a need to inform visitors about upcoming special events and businesses in the downtown. There are a wide range of digital street kiosks available (some with interactive touch screens) which could address this need.

A digital kiosk could provide wayfinding information as well as opportunities to advertise future special events and businesses.



Figure 42 - Steps and steep grade area a barrier to many people who want to walk south on Washington Avenue

Movable outdoor chairs could be tested in the downtown. People will almost always select a movable, individual chair over a fixed bench because it allows them to place the chair in the sun or shade and in groups as needed. Placing a small number of relatively inexpensive outdoor chairs in an appropriate location on Washington Avenue would determine if they function as desired and if they would be subject to vandalism or theft. If proven to be successful, additional groupings could be added.



Figure 43 - City maintenance staff noted that solar powered compacting trash and recycle receptacles could reduce maintenance costs





Figure 44 - Flex space showing parking space used for a dining deck (Temporary curb filled in with temporary brick pavers)



Figure 45 - A strong street tree canopy enhances user comfort

## WASHINGTON AVENUE BRICK AND STONE CORNER PLANTERS

It appears there is no middle ground on the downtown's brick and sandstone corner planters. During the open house and online review, the support to keep the planters "the way they are" was as strong as the support to make some type of change to "create more usable space". Among the downtown stakeholders, there was very strong support to reduce the scale of the planters. This may be an instance where the City looks at implementing a test case for revising a corner planter.

When a merchant or property owner makes a request to City Council to eliminate some, or all of a corner planter:

- ↪ A proposal should be submitted with details on the design changes. For example, how is the inside of the planter that was in contact with the soil treated if only a portion of the planter is removed?
  - Based on feedback from those who favored changing the planters, the preferred approach would be to remove half of the planter (Option A) or all of the planter and install flowering ornamental trees in tree grates (Option C).
- ↪ A sign should be posted advising residents that the change is being considered by Council.
- ↪ The costs should be the responsibility of the property owner and the proper permits secured.
- ↪ Consider a policy will that defines what happens if the business fails. Does the planter get restored; and, if so, who pays for it?
- ↪ It's worth noting that not every corner in the downtown has a planter, so introducing an atypical condition for one, or some of the planters is not without precedent.

## WASHINGTON AVENUE CLEAR CREEK TO 14<sup>TH</sup> STREET

When it came to the core of Golden's downtown, the feedback received from the residents and the downtown community on the existing streetscape was clear on a number of points:

- ↪ People loved the quality of the streetscape materials and details.
- ↪ They enjoyed the wide sidewalks but felt they were too cluttered at times with sandwich boards, signs, art, etc.
- ↪ They liked having seating available for outdoor dining.
- ↪ There was consensus that the benefits of street trees outweighed blocking retail signage.
- ↪ Many felt the pedestrian crossing signals were inefficient.

During the review of the four options for changing the streetscape, there was less consensus on how best approach improving the downtown.

- ↪ There was support for creating more opportunities for outdoor dining. People liked the idea of converting parallel parking spaces to dining via wood or brick decks. Although, there was some concern about mixing parking and outdoor dining.
- ↪ Reducing emphasis on the car in the downtown was thought to be beneficial, even to the point of narrowing the width of travel lanes to slow traffic.
- ↪ Creating space to add more trees was viewed positively.
- ↪ Making the width of the walks equal was well supported.
- ↪ Given the pattern of responses received, there appeared to be hesitation to make radical changes to Washington Avenue.

Based on the feedback received and the technical challenges with storm drainage on Washington Avenue, it appears implementing the enhancements illustrated in Concept A would improve the downtown's streetscape and be supported by the community. Choice of this option as a model would also recognize the opportunity to distribute resources to continue improving all aspects of downtown. Alternately, the City could certainly choose one of the other options with more extensive changes and work to a longer term implementation. Selection of Option A as the overall model for Washington Avenue would:

- ↪ Create "Flex" space by converting the asphalt paving in the parallel parking spaces to brick or concrete unit pavers. Planning or making this change when Washington Avenue's paving is being resurfaced in



the next couple of years will reduce the cost, as the money the City would spend on new asphalt could go toward part of the paver cost.

- Reduce the width of the north / south travel lanes and center turn lane to slow traffic and create a Flex space on the east side of the street. This would help make the public space more equal by providing more opportunities for public seating, dining decks, and bike parking on the east side.
- Eliminating the sidewalk canopies or supporting canopies from the buildings (or post with structural support placed close to the buildings) should be a priority to reduce obstructions in the sidewalks.
- Implement other changes described in this section to add more seating in the downtown and reduce the scale of the corner planters where appropriate.

The City should consider commissioning a detailed engineering study of the storm drainage system in the downtown to see if it is feasible to implement more extensive changes on Washington Avenue in the future. Moving the curb line on the east side of Washington Avenue to construct 17' wide sidewalks or implementing the "Shared Street" idea shown in Concept D could be long term improvements worth considering for Washington Avenue if the drainage challenges can be addressed.

### JACKSON STREET PAVEMENT TO PARK

While not strongly supported during the design charrette, the pavement-to-park concept was better received during the open house / MindMixer review. If detailed design studies identify an attractive and functional set of improvements that can be implemented cost effectively, the pavement-to-park concept for Jackson Street could have a positive impact on the East Downtown. It would attract people to the blocks east of Washington Avenue and provide another activity area for the Downtown. It would also help soften the unattractive, blank west face the Wire Transfer building.

### EXPANDING BEYOND WASHINGTON AVENUE

As properties in the East Downtown redevelop, the streetscapes should be planned to encourage a vibrant public realm with:

- Wide sidewalks that repeat the materials and details from the downtown.
- Public plazas on the private properties that front on the street which are large enough to accommodate outdoor dining and retail displays.
- Curb extensions with landscaping and enhanced paving at each corner.
- Pedestrian scale lighting.
- A strong canopy of street trees.
- The inclusion of uses that generate activity such as interactive/kinetic art or small children's climbing sculptures.
- On-street parking (parallel or angle) with enhanced paving to create "Flex" space.
- Narrow traffic lanes to slow vehicles.
- Full street closures could be considered if appropriate for the adjacent uses and compatible with the overall traffic flow in the downtown.

### OTHER CONSIDERATIONS

Attracting shoppers to explore beyond Washington Avenue will be essential to growing the downtown. Creating visually attractive places to visit or active features that can be seen from Washington Avenue will be needed.

- Creating a focal point on the southwest corner of Jackson and 12th Streets could entice people to venture off Washington Avenue. A tall, colorful interactive art piece or interactive water feature could be such a focal point. Night lighting will also be an important consideration if this element is to be effective.
- Miners and Prospectors Alleys have an intimate scale and would be well suited for liner retail or small boutique shops. To accomplish this service and delivery vehicles would not be able to use the alleys during prime business hours, the dumpsters would need to be consolidated or very well screened, utilities buried, and the paving and streetscape character improved.
- Redevelopment of underused properties will be critical to the long-term growth of the downtown and should be encouraged. Implementing recent proposals to GURA for developing the vacant lots north and south of the Jackson Street parking structure into mixed use would be an important first step in this process.



Figure 46 - Colorful public art can create a focal point and encourage pedestrians to explore a new area



Figure 47 - A small street-corner splash pad could invite patrons to explore other areas of the downtown







